



RESIDENCES AT SILVERCREEK SITE PLAN

Planning Report
July 2020



RESIDENCES AT SILVERCREEK

PLANNING REPORT

TRAVIS & ASSOCIATES

File: 2.295

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EXECUTIVE SUMMARY

The subject lands have municipal addresses of 11403, 11453 and 11461 Highway 26, Collingwood, Ontario. Legally the lands are described as comprising parts of Lots 47 and 48, Concession 10. All three parcels are now legally and beneficially owned by 1964245 Ontario Ltd. As each parcel consists of whole lots and not considered parcels, they have merged under provisions of the Planning Act.

Site Plan Approval is sought for a comprehensive site plan application on an existing lot for 260 dwelling units. The proposed 260 units comprise 200 apartments in two, four storey buildings and, 60 bungalow style townhomes.

The 5.1ha site is irregular in shape and has a frontage of approximately 183 m along Highway 26. The site is partially vegetated and for the most part slopes gradually to the north east.

The subject lands clearly lie in a serviced area designated for residential development in a medium density category that permits townhomes and apartments to a maximum density of 55 units per ha (upha). The comprehensive site plan will result in 200 apartments and 60 townhomes resulting in a density of 53.5 upha. In terms of land use, the subject application conforms to the Official Plan direction.

Throughout an extensive preconsultation process with the municipality the proposal has undergone design refinements. These refinements coincide with the town's emphasis on quality urban design. The Urban Design Brief identifies key design elements that account for directions in the Town's Urban Design Manual (UDM), in particular building form, architectural style, height mitigation measures, traffic and pedestrian circulation, landscape treatments and, respect for the predominant neighbourhood context. Likewise, the Urban Design Manual Compliance Report describes how the proposal meets the requirements of the UDM.

It is demonstrated that the subject application conforms to the intent of the Official Plan and complies with the zoning By-law regulations. It is also demonstrated that the proposed plans comply with the intent and specific objectives of the UDM and represents an example of quality urban design which merits support from the Town.

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1. PURPOSE

The purpose of this PJR is to present and assess the Skydevco Site Plan Approval application filed under the project name of “Residences at Silvercreek”. In particular, this report will:

1. Explain the planning approval proposal
2. Describe the existing land use context
3. Identify applicable land use planning policy considerations
4. Review supporting technical documents
5. Provide a professional planning opinion and,
6. Summarize findings

2. SITE DESCRIPTION

The subject lands have municipal addresses of 11403, 11453 and 11461 Highway 26, Collingwood, Ontario. Legally the lands are described as comprising parts of Lots 47 and 48, Concession 10. The three municipal addresses reflect the historical use of the subject lands for three single detached dwellings on three separate lots. Two of the dwelling had direct access of the northerly highway frontage and the third had access directly off the highway along the easterly frontage. The dwellings were vacated several years ago and have since been demolished. The subject lands are vacant.

All three parcels are now legally and beneficially owned by 1964245 Ontario Ltd. As each parcel consists of whole lots and not considered parcels, they have merged under provisions of the Planning Act. They cannot be sold independently (separately, apart from one another) without proceeding through a severance process under S. 53 of the Planning Act.

The 5.1ha site is irregular in shape and has a frontage of approximately 183 m along Highway 26. The site is partially vegetated and for the most part slopes gradually to the north east. A smaller portion of the subject lands drain to the west. Detailed descriptions of the existing conditions of the site are provided for in the July 2020 servicing and environmental reports of C.F.Crozier and Associates as submitted with the subject application.

3. SURROUNDING LAND USES

The immediate neighbourhood can be characterized as predominantly comprising medium density type residential dwelling units. The residential character of the medium density types in the area is generally that of a two or three storey condominium. The one exception in this medium density description would be the ten-storey Ruperts Landing condominium located to the north-east.

More immediate land uses include the adjacent Wyldewood condominium (160 units, three storeys) to the south, the municipal water reservoir and Highway 26 to the north, a singled detached residential

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dwelling to the north west and open space and the Cranberry Marsh feature to west (see Figure 1, over). Existing trails traverse the south-west corner and serve as a connecting link between public trails established through the Wyldewood development process and, a public trails system in the Town's open system component of the Cranberry Marsh feature to the west.

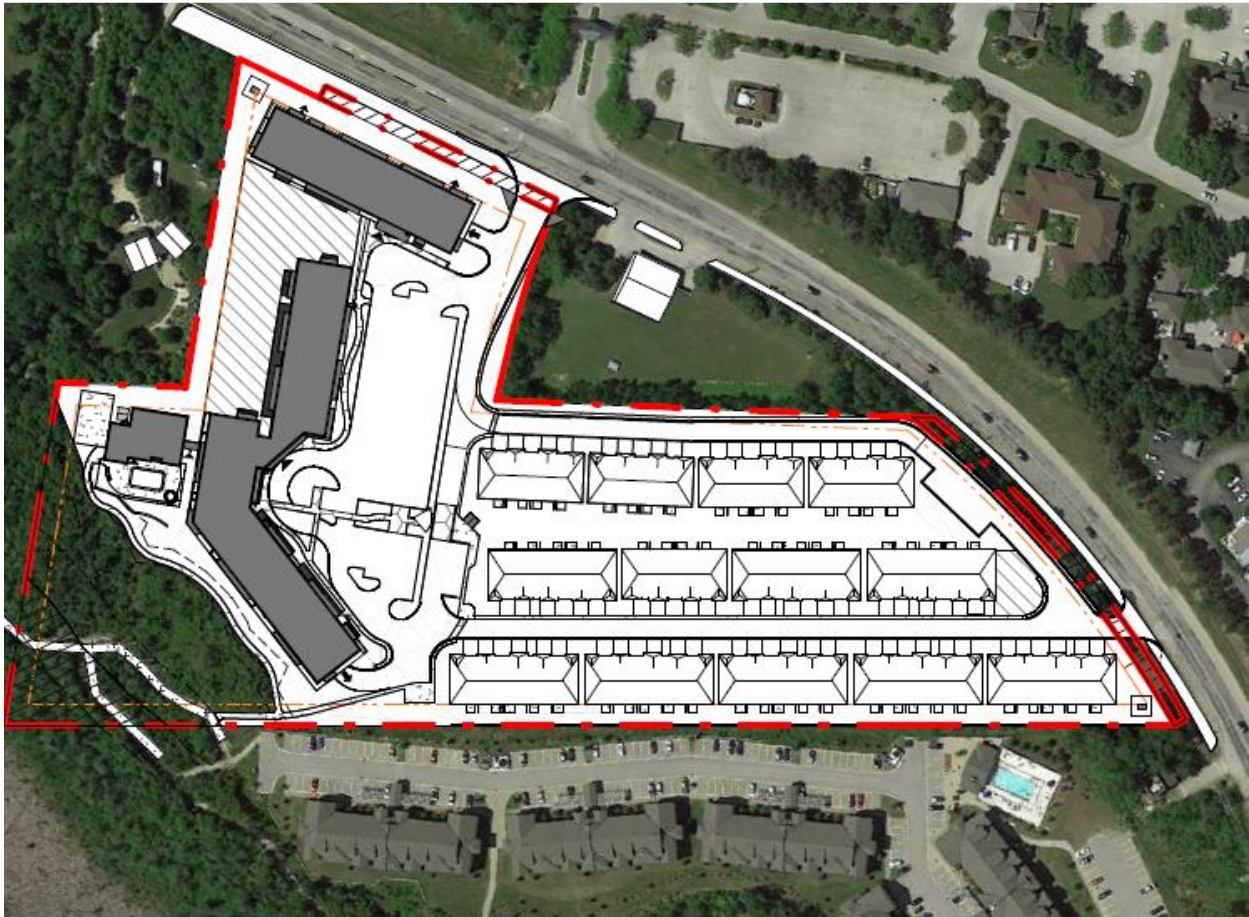


Figure 1: Immediate Site Context

4. PROPOSAL

The proponent is seeking Site Plan Approval for comprehensive site plan on an existing lot for 260 dwelling units. Architectural, site plan and landscape plan details are included with this submission. Additional commentary on site context, design elements and related commentary are provided for in the attached Urban Design Brief prepared by SRM Architects.

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4.1 The Basic Site Plan Numbers

The proposed 260 units comprise 200 apartments in two, four storey buildings and, 60 bungalow style townhomes. The below site plan statistics are taken from SRM Architects Inc drawing A1.1 as issued for Site Plan Approval. Table 1 is an excerpt of the Site Data component.

In Table 1 the gross lot area is reported at 5.1ha. To arrive at the equivalent density for Official Plan conformity exercise purposes, the road widening along with the lands that are designated “EP” (Environmental Protection) are subtracted from the gross area. The resulting area available for density calculation purposes is 4.86ha. This is referred to as the “net lot area” in the table.

SITE DATA			
11403, 11453, & 11461 Hwy 26 W., Collingwood			
DATA		REQUIRED	PROVIDED
ZONING		ZONING - R3-33	
LOT AREA (m ²)		50,974(m ²) (5.1ha.)	
ROAD WIDENING (m ²)		1,047(m ²)	
EP ZONED LANDS (m ²)		1,364.93 (m ²)	
NET LOT AREA (m ²)		48,562(m ²)	
MIN. FRONTAGE (m ²)		30 (m)	183.4 (m)
SETBACKS	FRONT YARD (m)	7.5 (m)	7.5 (m)
	INTERIOR SIDE YARD (m)	7.5 (m)	7.5 (m)
	EXTERIOR SIDE YARD (m)	4.5 (m)	4.5 (m)
	REAR YARD (m)	7.5 (m)	7.5 (m)

Table 1: Basic Site Data

The proposed density is calculated at 260 dwelling units divided by 4.86 ha. This results in an overall site development density of 53.5 units per ha. This is under the maximum density permitted in the Official Plan of 55 upha (which would total 267 units).

The Table 1 also identifies lands set aside for road widenings along Highway 26, which are accounted for in the comprehensive site plan layout and design.

Table 1 also shows that the proposal exceeds the minimum required frontage and maintains the minimum yard setbacks. With regards to exterior side yard references, the table above applies the townhouse R3 zone exterior side yard setback of 4.5m.

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Table 2, below provides further site plan statistics in the form of summarized building data, and is also sourced from SRM Architects.

Key information in Table 2 confirms that the proposal would result in slightly fewer units than permitted using the maximum density of 55 upha (seven units fewer). In addition, the number of storeys is set at 4 with a maximum height of 14.75m. This is less than the maximum 15m height limit set under the R3 zoning provisions.

In addition, the Building Data table shows that the building coverage is at 29.4%, well below the zoning By-law maximum of 40% in the R3 zone.

BUILDING DATA		
DATA	REQUIRED	PROVIDED
TOTAL DENSITY (# of units/NET ha.)	20 MIN. x 4.86 ha- 97 55 MAX. x 4.86 ha- 267	260 (units) 200 apt. units 60 townhomes
COVERED PARKING AREA (m ²)	--	7,842(m ²)
BUILDING 1 AREA (m ²)	--	1,459.12 (m ²)
BUILDING 2 AREA (m ²)	--	3,148.41 (m ²)
TOWNHOMES BLDG. AREA (m ²)	--	5,632.8 (m ²)
AMENITY BUILDING AREA (m ²)	--	419 (m ²)
TOTAL BUILDING AREA (m ²)	--	13,893.8 (m ²) (covered parking + amenity + townhomes)
BUILDING COVERAGE (%)	40 (%) MAX.	29.4% (covered parking + amenity building+ townhomes)
GROSS FLOOR AREA BUILDING 1 (m ²)	---	5,836.48 (m ²)
GROSS FLOOR AREA BUILDING 2 (m ²)	---	12,593.64 (m ²)
NUMBER OF STOREYS	---	4
BUILDING HEIGHT (m) (parking level to top of roof deck)	15(m) MAX.	14.75 (m)
AMENITY AREA (m ²)	---	304.77 (Townhome area- outdoor) (m ²) 419 (apartment building) (m ²)

Table 2: Building Data

The landscaped area in the comprehensive site plan is 43.8% and is over the minimum R3 zone requirement of 40%.

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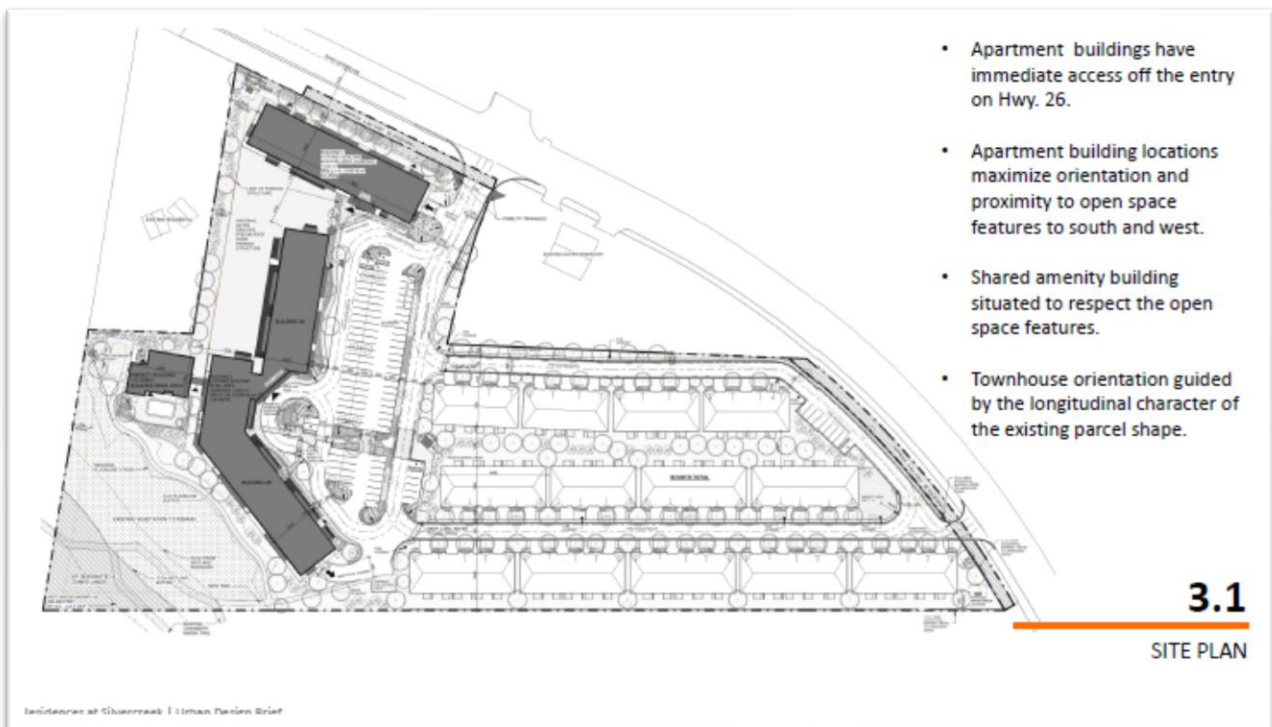
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Parking required under the R3 zone provisions totals 370 spaces. This accounts for 250 spaces for the 200 apartment type units and 120 spaces for the Townhouse type units. Parking supply proposed is 414, the sum of 279 parking spaces allocated to the apartment dwelling uses and 135 spaces allocated to the townhouse dwelling uses.

In addition, the site plan provides a total of 20 bicycle spaces.

4.2 The Plans

Full sets of architectural, engineering and landscape plans are submitted with this application. Additional reference should be made to the Urban Design Brief (prepared by SRM Architects) and the Urban Design Manual Compliance Report (prepared by Travis & Associates) also submitted with this application.



Site Plan

The plans support a proposal for a comprehensive master site plan. Main plan elements are integrated through a road system accessed off Highway 26 combined with an internal sidewalk and trail system. The internal road system includes provisions for an emergency access at the east and connected to Highway 26. The landscape plan provides further integration through consistent landscape treatments that define site edges and key open space elements as well as mitigate visual impacts.

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Height mitigation is addressed through additional building setbacks and plantings. Height mitigation measures are detailed in the submitted plans. Architectural features also assist in this respect. The proposal conforms to the angular plane design guideline addressing building setbacks.

BUILT FORM PRINCIPLES



- Architecturally varied and context appropriate streetscape that follows the Contemporary New Urbanism as outlined in the Urban Design Manual.
- Design incorporates traditional materials widely used in the immediate area in a contemporary way, such as ledge rock, stone, and wood tonalities.
- Façade articulation carried through framing of the terraces and balconies as well as the ledge rock wall enclosing the parking area. Along with landscaping features, all elements are combined to create an attractive streetscape along Hwy. 26.
- Terrace and balcony placement maximize the vistas to the surrounding landmarks – Blue Mountain and Georgian Bay, existing wetland, and Cranberry Marsh Trail.

3.0

DESIGN OVERVIEW

Residences at Silvercreek | Urban Design Brief

Design Overview

The 60 bungalow style townhomes located in the east section. The architectural style borrows from the Contemporary New Urbanism. The property boundaries in this part of the site are characterized as being oblong in shape. A grid type internal street pattern off the main entry road allows for an efficient use of land with an elongated crescent road approach. The emergency access to Highway 26 is at the south-east part of this section. The southerly buildings back onto with the adjacent three storey Wyldewood condominium project to the south.

This sector of the site is integrated into the development through not only the road connection but a sidewalk system. Connectivity is further reinforced through substantial street landscaping tying into the apartment sector to the west.

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Bungalow Townhome Streetscape Design Approach

5. BACKGROUND

An initial Preconsultation Meeting was held on July 26, 2018 (Town file D00116) to consider a 267-unit condominium development proposal on the subject lands. The proposal called for two, four storey apartment buildings: one with 187 dwelling units, the second with 80 dwelling units.

The development proposal was refined as the current proponent, Skydevco, updated the development approach to consider rental apartment. This update was reviewed through a second Preconsultation Meeting, held on October 16, 2019. The proposal remained at 267 dwellings but at 193 apartment type units and 74 townhouse type units.

A third Preconsultation Meeting was held January 22, 2020 with more detailed plans, studies and reports addressing comments from the October 16, 2019 meeting.

In March, 2020 Planning Staff issued a consolidation of comments to our application heard at the January 22, 2020 meeting. As a result, additional detailed work and study were undertaken including advanced engineering, architectural and design. A new architectural firm was introduced to the project with building and site plan elements seeing positive steps forward.

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A comments response table is provided this Planning Report and summarizes how the consolidated comments provided in March 2020 have been addressed (see below).

5.1 Preconsultation with Town: Comments Response

Appendix 2 to this report summarizes in table form the preconsultation process comments provided by Planning Staff in March 2020.

In referencing Appendix 2 Preconsultation Comments and Response Table, it is seen that agency comments have been addressed. For the most part this is accomplished through the detailed reports and studies submitted along with revisions and updates to proposed plans submitted with this application. Design discussion is provided in the submitted Urban Design Brief and the Design Manual Compliance Report.

A key agency response that has resonated throughout the consultation process with the Town has been that of the NVCA with regards to the EP lands in the south-west corner that are associated with the Cranberry Marsh feature. In addressing these comments, the EIS acknowledges the wetland boundary and the existing 15m buffer established within the existing EP designation and zoning. The EIS observes that an additional natural no touch buffer area is identified for protection due to the established flood line limit and its associated additional 6m buffer. These flood line setbacks add a further 45 to 55 m setback (and an effective buffer) from the established wetland boundary.

The EIS correctly notes that any site disturbance from development will be located more than 30 m (at the closest point) from the wetland boundary. With no such disturbance within the flood line setback along with the mitigation measures addressing site protection and management of site stormwater the EIS concludes that no negative impacts on the PSW will result. The EIS concludes that the existing 15m buffer and EP has been zoned appropriately.

7. PLANNING POLICY CONSIDERATIONS

The Lands are in Collingwood, Ontario and are therefore subject to several layers of policy direction. Although the primary policy direction in considering approval details lies in the local Official Plan, municipal decisions on planning matters do need to demonstrate consistency with Provincial policy and Plans.

The two provincial level policy instruments are the Provincial Policy Statement, 2020 (“PPS”) and, the Growth Plan for the Greater Golden Horseshoe, 2019 (“Growth Plan”).

The two municipal policy instruments are the County of Simcoe Official Plan (the County Official Plan) and, the Town of Collingwood Official Plan (the Town Official Plan).

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7.1 Provincial Policy Statement

The 2020 Provincial Policy Statement (PPS) came into effect May 1, 2020 and replaced the previous 2014 PPS. It remains a province wide policy directive on matters of provincial interest in land use planning. Planning authorities are to make decisions on planning matters that are consistent with the PPS.

In reviewing the PPS the following represents a summary of Part V policy areas that are particularly relevant to the subject application and proposed development.

Section 1: As the subject applications would enable development in a primary settlement area on full municipal services and contribute to the available range of dwelling types in the municipality, it is concluded that the applications are consistent with Section 1.1 (efficient and resilient communities).

Extensive study and consultation concerning the subject lands to date, along with the subject application and support materials provided demonstrate consistency with Section 1.2 (coordination).

Likewise, the subject applications are consistent with Sections 1.4, 1.5 and 1.6 due to location, nature and scale of the development (housing, open spaces, infrastructure). Development of the subject lands will result in adding to the mix of available housing, maintain its contribution to the municipal open space systems and enable continued efficient servicing due to housing density, type, location and implementation of municipal engineering standards.

Section 2: The subject applications are consistent with Sections 2.1 and 2.2 (natural heritage and water) as natural features and their boundaries are confirmed through the EIS to be in accordance with the existing Official Plan environmental protection designation. This position is arrived at through the preparation of updated environmental work in accordance with the requirements of the NVCA. The subject applications are consistent with Section 2.6 as a Stage 1 and Stage 2 archaeological investigation and study were completed and accepted as having no archeological resources or potential areas of interest.

Section 3: The regional flood line was established through additional engineering study. Proposed buildings are situated outside of the regional flood line. The subject application is consistent with Natural Hazards policy.

Concluding Comment: PPS

Overall, given the location of the subject lands and nature of the proposed uses, the subject application proposes development that is consistent with PPS directions. The proposal supports efforts an efficient use of land by way of housing form and density, adds to broadening the inventory range of housing types, promotes the efficient use of municipal infrastructure while adding supportive transit uses due to location and housing types and, enables active transportation initiatives through existing and potential pedestrian connections. The completed EIS identified environmental features and confirmed protection and mitigation measures.

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7.2 Growth Plan (2019)

The Town of Collingwood lies within the boundaries of the Growth Plan for the Greater Golden Horseshoe (“Growth Plan”). Schedule 8 to The Growth Plan designates the Town of Collingwood as a “Primary Settlement Area” within the “Simcoe Sub Area”. Generally, the Growth Plan provided further, specific policy detail under the umbrella policies of the PPS. Policies in Sections 2, 3 and 4 have a bearing on the subject application.

Section 2, “Where and How to Grow” provides directions on where growth is to occur and the components of “Complete Communities” that growth should contribute to and support. This includes policies supporting provision of a range of housing types on municipal services. As these policies are aligned with the PPS key policy areas such as 2.2.1, Managing Growth that direct where the majority of growth is to occur, the contribution of growth to maintenance and development of complete communities are implemented through the subject application by virtue of location, type and form.

Likewise, key policy 2.2.6 – Housing, provides direction on achieving complete communities through housing location, diversity, intensification and form. The subject application will result in development that will assist the municipality in achieving these policy directions and objectives.

Although policies in 3.2 are primarily geared to municipal planning and investment in infrastructure, the subject application contributes these directions through housing type and location (density, and within a serviced area) along with supporting studies identifying municipal infrastructure availability, connections and contributions.

Section 4 policies in the Growth Plan that are particularly applicable to the subject application concern hydrologic and natural heritage systems. The submitted EIS along with engineering studies addressing drainage and hydrology have identified and confirmed key hydrological and natural heritage systems features of the site and adjacent areas. The findings conclude that the proposed development would not have a detrimental impact on functions and features. In addition, Section 4 policies address cultural heritage resources. In this respect, the completed and accepted archeological studies confirm no presence of such resources on the subject lands.

Concluding Comment: Growth Plan

As a result of the specific location of the subject lands in combination with the nature, scale, use and form of the proposed development, it is concluded that the subject application is consistent with the policy direction of the Growth Plan.

7.3 Simcoe County Official Plan

Generally, policy directions provided in the Simcoe County Official Plan are further refined in the Collingwood Official Plan. Nonetheless, the following summary is provided as County policy context in considering the subject application.

Schedule 5.1, “Land Use Designations”, to the County of Simcoe Official Plan designates the subject lands as being within the “Built Boundary” of the Primary Collingwood Settlement Area.

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Section 3.5 contains policies addressing Settlement Areas. Overall, the objectives are to focus growth and development within settlements in a location and manner that provides for efficient, compact urban form, on full municipal services contributing to mixed use settlements.

Policy 3.5.24 directs that Collingwood is to achieve a 40% intensification target within the built boundary.

Concluding Comment: Simcoe County Official Plan

The subject lands are located within Built Boundary in a Primary Settlement Area. The proposal will result in residential uses on full municipal services and at a form and density that should assist the municipality in achieving its 40% intensification target. With regards to these key policies, the subject application conforms to the intent and direction of the County of Simcoe Official Plan.

7.4 Town of Collingwood Official Plan

The Town of Collingwood Official Plan (Official Plan) was approved by Simcoe County in May 2007. In February 2015, Official Plan Amendment 33 was approved by Simcoe County. This Amendment updated growth policies including the Land Use Schedule and the Residential Density Schedule.

There are two general areas of Official Plan policy that are of particular relevance to the subject application. The first includes policies specific to development land use, location, and scale. The second, concerns form and design.

Schedule 'A', Land Use Plan, designates most of the subject lands as "Residential". A smaller part of the south-west corner is designated "Environmental Protection". In general, the Residential designation anticipates a range of residential dwelling types and notes that Schedule 'C' provides further policy on density ranges and unit types.

With regards to the remaining Official Plan Schedules, the following are noted:

- Existing Schedule 'B', Environmental Protection, identifies a part of the south-west corner of the subject lands as "Category 1 Wetlands". This feature is associated with the Cranberry Marsh situated to the west of the subject lands.
- Existing Schedule 'C', Residential Density designates the subject lands "Medium Density Residential". Section 4.3.2.6.2 stipulates that the minimum density under this designation is 20 units per ha (upha) and a maximum density shall be 55 upha. Section 4.3.2.6.1 stipulates that the range of residential uses shall be limited to single detached, semi-detached, duplex, fourplexes, triplexes, townhouses, apartments, student dormitories and boarding homes.

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- Schedule “D”, Transportation Plan, shows the subject lands front onto an “Arterial” road, that is Highway 26.
- Schedule “D1”, Trail System, shows a “Future Pedestrian Trail” along the Highway 26 frontage and, to the southwest, linking existing development to the south through to the Cranberry Marsh lands to the west and beyond.
- Schedule E, “Municipal Service Areas” shows the subject lands located in Service Area 1, indicating a preferred development phasing area.
- Schedule “E1”, Municipal Services Plan, shows Water and Sanitary Trunk lines running along the Highway 26 corridor that abuts the subject lands.
- Schedule ‘F’, Urban Structure identifies the subject lands as being within the “Inside Built Boundary” area.

Section 4.1.3.3 addresses the matter of boundaries of environmental designations. The intent is to allow for minor modifications to the boundaries “provided the overall intent of the Plan is maintained”. No modifications are being proposed.

Section 4.3.1.2 provides general residential development policies that includes objectives under the heading “Quality and Choice”. One such policy is to “establish and maintain comprehensive urban design and development standards for new residential development ...”. Section 4.3.2.2, Urban Design, further states:

This Plan recognizes that high quality urban design is essential to achieving compact and complete communities, helping to create an attractive, accessible, walkable and safe built environment.

The Town’s Urban Design Manual provides standards for achieving high quality urban design and it is the intent of this Plan that these standards will form an integral part of the review of residential development proposals throughout the Town.

Of particular importance is looking at how urban design can provide for a greater degree of flexibility and innovation in addressing potential conflicts between land uses, housing types and densities, and how it can contribute to enhancing existing and proposed neighbourhoods.

Section 8 to this report, below, reviews the subject application in light of the Collingwood Urban Design Manual (UDM) and concludes that that the proposal complies with the directions provided in the UDM. In addition, the subject application includes an Urban Design Brief that provides additional design reference and context.

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Concluding Comment: Collingwood Official Plan

The subject application seeks site plan approval for a comprehensive site plan design that will result in medium density residential uses on full municipal services and within an area having a predominant medium density residential character. In terms of land use type and density the subject application conforms to the Official Plan. It is concluded that no changes to the land use boundaries or designations are required.

The Official Plan also provides policy direction reinforcing quality urban design for new residential development and relies on the Urban Design Manual as a further instrument in the review and assessment of development proposals. The subject application is reviewed under the Urban Design Manual Compliance Report provided with this application. This review demonstrates how the subject application complies with the directions sought in the UDM.

In summary, the subject lands clearly lie in a serviced area designated for residential development in a medium density category that permits townhomes and apartments. The comprehensive site plan will result in 200 apartments and 60 townhomes resulting in a density of 53.5 upha. In terms of land use, the subject application conforms to the Official Plan direction. Compliance with the UDM demonstrates further conformity with the intent of the Official Plan.

8.0 Urban Design

Urban design was identified as a key consideration during the preconsultation process with the municipality. The initial site plan layouts and architectural approaches were reviewed by the proponent. The proposal submitted with this application represents a refinement over previous plans by taking into account additional design considerations addressing building layout, height mitigation measures and building designs.

These considerations are detailed in the Urban Design Brief and the Urban Design Manual Compliance report provided with this submission.

It is concluded that the subject application complies with the Town's UDM.

9.0 SUPPORTING STUDIES and Materials

As a result of preconsultation processes a comprehensive list of studies and materials were identified as being required. This list is attached as Appendix 1 to this report. Below are summaries of findings found in six of the main studies (submitted with this application).

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Table 8 to the report provides a summary of potential impacts to natural heritage features and proposed mitigation measures to address the potential impacts. With regards to the EP feature (PSW) to the southwest the EIS notes that any site disturbance from development will be located more than 30 m (at the closest point) from the wetland boundary. With no such disturbance within the flood line setback along with the mitigation measures addressing site protection and management of site stormwater the EIS concludes that no negative impacts on the PSW will result. The EIS concludes that the existing 15m buffer and EP has been zoned appropriately.

In summary, general recommendations and mitigation measures reinforce the need for protective fencing to be installed as outlined in the tree protection plan prior to construction commencement, installation of sediment protection fencing, control of overland flows, designated locations for soil stockpiling, no disturbance of vegetation in identified protection areas, limits on construction activity during the May to June breeding bird season, and, dark sky compliant lighting to reduce potential light spill into the adjacent PSW.

Servicing and SWM Implementation Report

The engineering report reviewed servicing and stormwater management requirements and concluded that the subject lands and proposal can be serviced.

The primary site access off Highway 26 is backed up by an emergency access to the Highway in the southeast corner. Utilities are available and can be extended to service the site. Internal sanitary services can be connected to the existing gravity trunk under Highway 26 right of way. Likewise, internal water distribution will connect to an existing main in the Highway 26 right of way with looping to a planned stub from the Wyldeewood development to the south.

Stormwater quality and quantity control measures are recommended. The Cranberry Marsh regional flood elevation of 179.65 is accounted for in the site plan with a 6m horizontal buffer and all building elevation to be 0.3m above the regional flood elevation. The apartment and townhouse basement slab are set at higher elevations based on ground water monitoring report recommendations by PetoMacCallum

Geotechnical Investigation

Site investigations and testing were undertaken. The report acknowledges the need to raise the site elevation to address ground water levels (a situation not uncommon in Collingwood). Recommendation include replacing existing topsoil with engineered fill. Standards for basement floors and walls are provided. Pavement design and construction recommendations are also provided. Ongoing geotechnical review and testing and monitoring during construction should be ongoing.

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Traffic Impact Study (TIS)

The submitted TIS is an update to the 2008 study.

The proposed development is forecasted to generate 105 and 129 trips during weekday a.m. and p.m. peak periods. The report notes that the existing intersection is operating at good levels of service and will continue to do so with the addition of site generated traffic to the Highway 26 boundary road network to have minimal impacts.

The queuing analysis for the underground garage entrance concluded that under normal circumstances, the two-vehicle stacking would be sufficient to accommodate the average of one vehicle

The report acknowledges that the existing intersection is undergoing detailed design with a functional design submitted with this application.

Stage One & Two Archaeological Assessment (AA)

The AA concluded that the subject lands do not contain archaeological resources of cultural or heritage value or interest. In a letter dated September 20, 2008 the Ministry of Culture advised the consultants that the report was reviewed, and the findings agreed with.

Noise Feasibility Study (NFS)

The NFS concludes that to address transportation noise from Highway 26 two acoustical barriers will be required along the easterly boundary. One at 2.6m high and the other at 1.5m are detailed in the site plan submission.

10.0 SUMMARY/CONCLUSION

In addition to this planning report the subject applications are supported by several reports and studies addressing prescribed matters. In total, the various submissions support a proposal for residential uses in accordance with local Official Plan land use designations and zoning By-law provisions.

It is demonstrated that the subject application conforms to the intent of the Official Plan and complies with the zoning By-law regulations. It is also demonstrated that the proposed plans comply with the intent and specific objectives of the Urban Design Manual.

Throughout an extensive preconsultation process with the municipality the proposal has undergone design refinements. These refinements coincide with the town's emphasis on quality urban design. The Urban

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Design Brief identifies key design elements that account for directions in the Town's Urban Design Manual, in particular building form, architectural style, height mitigation measures, traffic and pedestrian circulation, landscape treatments and, respect for the predominant neighbourhood context. Likewise, the Urban Design Manual Compliance Report describes how the proposal meets the requirements of the UDM.

Taking into account the above considerations it is concluded that the subject application is consistent with the PPS, is consistent with the Growth Plan, conforms to the County Official Plan and, conforms to the Town Official Plan. Review of design approach and detail shows that the proposed plans comply with the intent and specific objectives of the Urban Design Manual. Consequently, it is respectfully positioned that the subject application is in the public interest, represents good planning and merits support and approval by the Town.

Prepared By:

A handwritten signature in black ink, appearing to read "Colin Travis". The signature is written in a cursive, flowing style.

Travis and Associates
Colin Travis, MCIP RPP

July 2020

RESIDENCES AT SILVERCREEK SITE PLAN APPLICATION
PLANNING REPORT
Travis & Associates
July 2020

APPENDIX 1

List of Required Materials, Reports and Studies

RESIDENCES AT SILVERCREEK SITE PLAN APPLICATION

PLANNING REPORT

Travis & Associates

July 2020

RESIDENCES AT SILVERCREEK SITE PLAN APPLICATION MATERIALS: TOWN FILE NUMBER D002719		
Travis & Assoc July 23, 2020		
REQUIRED MATERIALS (per March 2020 Planning Department Comments)	MATERIALS PROVIDED WITH THIS SUBMISSION	NOTES
ENGINEERING		
Civil Engineering Drawings	C.F. Crozier, July 2020	
Intersection Design Drawings	C.F. Crozier, July 2020	
Service and Stormwater Report	C.F. Crozier, July 2020	
Traffic Impact Study	C.F. Crozier, July 2020	
Hydrogeological Plan	Peto MacCallum, July 2020	
Geotechnical Report	Peto MacCallum, July 2020	
Active Transportation Plan		
Master Fire Plan	srm Architects, July 17, 2020	Routes, hydrants, connections identified on Site Plan
Photometric Plan	Fortech Engineering, July 2020	
Construction Management Plan	Reid's Heritage Construction	
Dust Control Measures	Reid's Heritage Construction	Provided as part of required construction management plan
Noise Impact Study	RWDI Air, Consulting Engineers & Scientists, July 15, 2020	
ENVIRONMENTAL		
EIS UPDATE (spring 2020)	C.F. Crozier/Azimuth, July 2020	
ARCHITECTURAL		
Site Plan	srm Architects Inc. July 17, 2020	
Building Elevations	srm Architects Inc. July 17, 2020	
Urban Design Brief	srm Architects Inc. July 17, 2020	
Shadow Analysis	srm Architects Inc. July 17, 2020	
Angular Plane Analysis	srm Architects Inc. July 17, 2020	Detailed in srm dwg No. A1.3
Height Mitigation Measures	srm Architects Inc. July 17, 2020	Described in Urban Design Brief
Architectural Design Guidelines (incl u/g pkg)	srm Architects Inc. July 17, 2020	See Urban Design Brief and Submitted Dwgs
LANDSCAPE ARCHITECTURE		
Landscape Plan	C.F. Crozier, July 2020	
Tree Preservation Plan	C.F. Crozier, July 2020	
Planning		
Completed Application Form	Travis & Assoc July 2020	
Application Cover Letter	Travis & Assoc July 2020	
Urban Design Manual Compliance Report	Travis & Assoc July 2020	
Planning Justification Report	Travis & Assoc July 2020	References UDM & UDB
Other		
Archeological 1 & 2	Timmins Martelle, Nov 2006	
Ministry Acceptance Letter	Ministry of Culture Sept 30, 2008	

APPENDIX 2

Comments and Response Summary Table

(appended separately)

RESIDENCES AT SILVERCREEK SITE PLAN APPLICATION

PLANNING REPORT

Travis & Associates

July 2020