

URBAN DESIGN BRIEF

PREPARED FOR:

Site Plan Control Application,
Official Plan and Zoning By-Law
Amendment

Charis Developments LTD.
869 Hurontario Street, Collingwood

File no. 21427A

July, 2025



Your Vision

Designed | Planned | Realized



PLANNING
URBAN DESIGN
& LANDSCAPE
ARCHITECTURE

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1.0 INTRODUCTION

MHBC has been retained by Charis Developments Ltd. (hereinafter as the "Owner") to prepare an Urban Design Brief for the development of lands located on the east side of Hurontario Street/County Road 124, north of Poplar Sideroad/County Road 32, and municipally addressed as 839, 853, and 869 Hurontario Street in the Town of Collingwood. (hereafter referred to as "the Subject Lands" or "the Site"). The purpose of the Urban Design Brief is to illustrate how the proposal implements the design guidelines found within the Collingwood Urban Design Manual and provide a comprehensive summary of how the proposed development addresses urban design matters.

Overall the proposal represents a mixed use residential, retail, office, service commercial and park space, providing a space with residents to live, shop and play.. The proposed development assists the Town in achieving the vision set forth for such areas, which is to provide a broader scope for the development of lands by maximizing developable area, accommodating development with optimized pedestrian and vehicular access, and encouraging reinvestment and revitalization of the existing built-up area. The proposal also provides for enhanced design and gateway landscaping, recognizing the Subject Lands are located on a central gateway and a major entry point to the Town.

THE POLICY FRAMEWORK

The subject lands are designated Highway Commercial and Residential, in accordance with Schedule A - Land Use Plan - of the Collingwood Official Plan and are located along Hurontario Street and Poplar Sideroad which are identified as Arterial Roads per Schedule D - Transportation. Hurontario Street, south of the town

limits, is under Simcoe County jurisdiction as County Road 124. Poplar Sideroad is under County jurisdiction as County Road 32. Schedule D1 - Trail System - identifies an existing trail and snowmobile route immediately north of the site along the Hamilton Drain Trail, with signalized trail crossing at Hurontario Street. The subject lands are designated Greenfield (lands for Urban Use) as per Schedule F – Urban Structure, which reflects its designation as Settlement Area, primary Settlement Area as per section 5.1.2 Simcoe Sub Area – Land Designations of the County to the Simcoe Official Plan. The Subject Lands are subject to the Town's Urban Design Manual ("UDM") to ensure that it is appropriately configured and designed to achieve high-quality design, landscaping, and entry features. Therefore, the following UDM sections are applicable: Site Layout, Buildings, Active Transportation, Landscaping & Public Spaces.

OUR APPROACH

In response to this design vision, MHBC, on behalf of the Owner, has prepared this Urban Design Brief to illustrate how the proposed development has responded to the design guidelines by the Town's UDM.

Should you have any questions or wish to discuss the brief in further detail, please do not hesitate to contact us.

Yours truly,

MHBC



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B.Arch, Cert. UD, Cert. PM
Urban Designer

2.0

HOW TO READ THIS BRIEF



This Urban Design Brief organizes key urban design principles into categories. Within each category, a written response demonstrating adherence with those principles is provided. In some cases where strict compliance is not feasible, design rationale is provided to outline how the design intent continues to be respected.

Well-designed developments can help to connect people with places, balance the protection of the environment with emerging built form, and achieve development that promotes a sense of place and local identity within a community. Key urban design terms have been used in this brief to further articulate how the proposal achieves good design principles and enhances the relationship with the surrounding community.

3.0

DESIGN VISION, GUIDING PRINCIPLES & OBJECTIVES

The development proposal aims to revitalize underutilized lands at the northeast corner of the Town's southern gateway by introducing a new mixed-use complex that aligns with the future planned context of the area while preserving its Highway Commercial character along the major arterial corridors. The proposal seeks to offer a diverse range of uses, including residential, office, retail and service commercial spaces, providing various services to residents and visitors from surrounding communities and the other parts of the Town. This development represents a high-level design and space programming effort that will enhance both the urban visual and functional character of this gateway.

The proposal will establish a compatible development that complements the planned context of the area while ensuring that the proposed facilities meet all functional requirements, including traffic management, parking, and servicing. It also incorporates high-quality landscaping near the adjacent public streets, within the parking lot, and around the proposed buildings to enhance the surrounding public realm. The development is designed to accommodate design guidelines and standards appropriate for a prominent mixed-use gateway plaza. The proposed site and building designs will achieve key design parameters such as legibility, accessibility, vibrancy, and safety, while also meeting functional requirements for screening and buffering.

The design of the buildings and material choices will establish an integrated and unique appearance that contributes to a visually appealing gateway, drawing inspiration from Collingwood's Lakeside and Mountain Recreational vernacular. The design of signage and landscaping elements will further enhance the character of the area, creating a distinct sense of arrival to the Town.

Additionally, the development proposes a permeable and accessible spatial layout that prioritizes pedestrian circulation both on and around the site. The proposed sidewalk will enhance the public realm, serving the buildings' frontages and providing direct access to the public sidewalk.

The massing of the buildings is designed at a human scale, creating a transition that corresponds to the Highway Commercial designation. Active frontages will animate and program the site, enhancing the pedestrian experience. Continuous building edges and articulation will frame the streewall, while vehicular and pedestrian circulation, along with site entrances, are designed and oriented to reflect the significance of the building's location and context within the area.

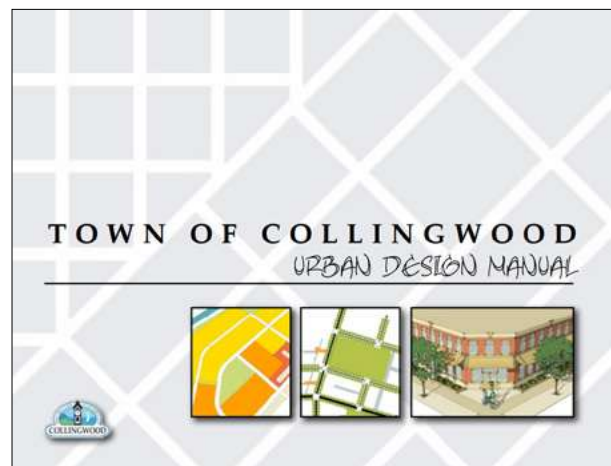


Figure 3.1 Render of the proposed development

GUIDING PRINCIPLES & OBJECTIVES

The proposal will achieve an overall compliance to ensure that the vision, goals, and objective of the Town's Official Plan and Urban Design Manual for Highway Commercial Areas are Implemented successfully through:

- Supporting Collingwood as a regional centre that serves residents, surrounding communities, and visitors,
- Promoting a unique, complete, and healthy community with a wider mix and diversity of uses, activities, and opportunities that responds to the evolving needs of the residents,
- Providing distinct, safe and an accessible urban environment at the southerly gateway of the Town, and along the major arterial roads,
- Promoting an urban design that is accessible, and pedestrian-oriented and facilitates local economy and social interaction,
- Ensuring efficient use of infrastructure, land and services by developing underutilized lands, and creating a quality development with a variety of uses and activities,
- Improving the physical design of the context area, streetscape and public realm by making them more attractive, conformable, and safer,
- Ensure a harmonious development that maintains the unique character of the area designated as Highway Commercial,
- Establishing an architectural and landscape design that contributes to design excellence and integration of the gateway by providing harmonious, integrated and high quality architectural products,
- Creating interesting, comfortable and safe pedestrian experiences,
- Optimizing safe and accessible vehicular circulation,
- Ensuring that landscaping, signage, screening, and lighting are designed with a coordinated theme.
- Defining gateway and entrance features through landscaping, decorative surface treatment, and other ornamental features.



4.0

CONTEXT ANALYSIS

The Site is located at the northeast corner of Hurontario Street and Poplar Sideroad intersection in the Town of Collingwood. The Simcoe Official Plan (SCOP) identifies the site as being within the primary Settlement Areas, lands designated to be developed as complete communities. Under the Town of Collingwood Official Plan, the site is designated as a Highway Commercial. The Subject lands are currently vacant.

The topography of the site is relatively flat, with a grade that generally falls from east to west. A treed area borders the northeast corner of the property.

The subject Lands are approximately 3.76 hectares in size, with 194.0 metres of frontage along Hurontario Street and 204.6 metres of frontage along Poplar Sideroad.

The Site is currently surrounded by the following uses:

North Immediately to the north is Hamilton Drain Trail, and further north are lands with a designation similar to Highway Commercial, which is currently occupied by detached houses.

East The lands directly to the east are low rise residential known as Pretty River Subdivision.

South Immediately to the south is Poplar Sideroad/ County Road 32, abutting the borders of The Town. The lands across the border are occupied by a wholesale (tree) nursery.

West Hurontario Street/County Road 124 abuts the Subject land in the west. Across the street, lands are designated as Highway Commercial, and are occupied

with commercial uses including MacEwen Gas Bar and Convenience, Tim Hortons, Walker's Small Motor, Wild Stand General Store, and a 2 storey office building. These lands also serve as a gateway location to the Town.

Part of the subject lands are regulated by the Nottawasaga Valley Conservation Authority ("NVCA"). The site is located within 120 metres of a stream, which runs parallel to the north side of the property, per Schedule 5.2.2 – Streams and Evaluated Wetlands of the SCOP. According to the comments during pre-consultation, NVCA staff has no natural hazard or heritage concerns with the proposed development. The location of this property also satisfies the 30m setback to wetlands from development activities.

The proposed development aligns with the Simcoe County Official Plan's vision for Settlement areas by offering commercial and service sectors that cater to the residential community in the south end of Collingwood, as well as to the travelling public. The proposal also adheres to the Town of Collingwood Official Plan Policies by supporting the planned context, complementing the area's character, and contributing a diverse range of activities that reinforce Collingwood's role as a regional service hub while enhancing residential and employment opportunities within both the Town and the broader region.

Furthermore, the proposal complies with the applicable guidelines of the Urban Design Manual by integrating high-quality architectural and site design. This results in a cohesive, integrated, and unique urban plaza that enhances the gateway experience, visually distinguishing the adjacent arterial corridors and marking them as a prominent entry point to the regional service area of Collingwood.

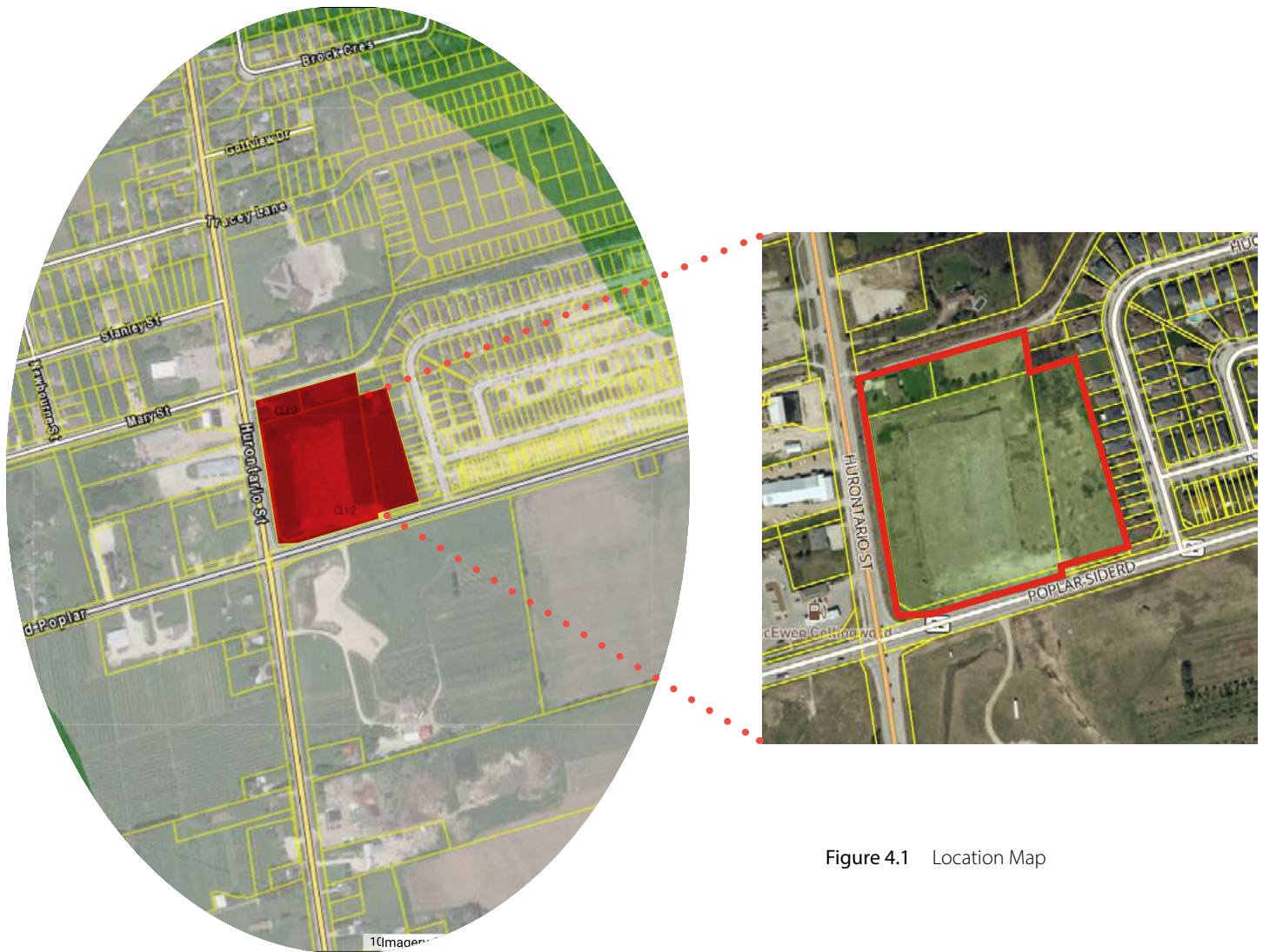


Figure 4.1 Location Map

EXISTING CONTEXT

The 800-meter context map shows a 10-minute walking distance surrounding the Site. This area occupies a wide variety of uses. The Subject Lands are connected to the Collingwood Harbour, Historic Downtown, and Regional

Transit via the existing waterfront and proposed trails system. Overall, the built form and character of the area are typically residential and commercial in nature along both arterial/county roads, with low-rise residential units located east of the subject lands.

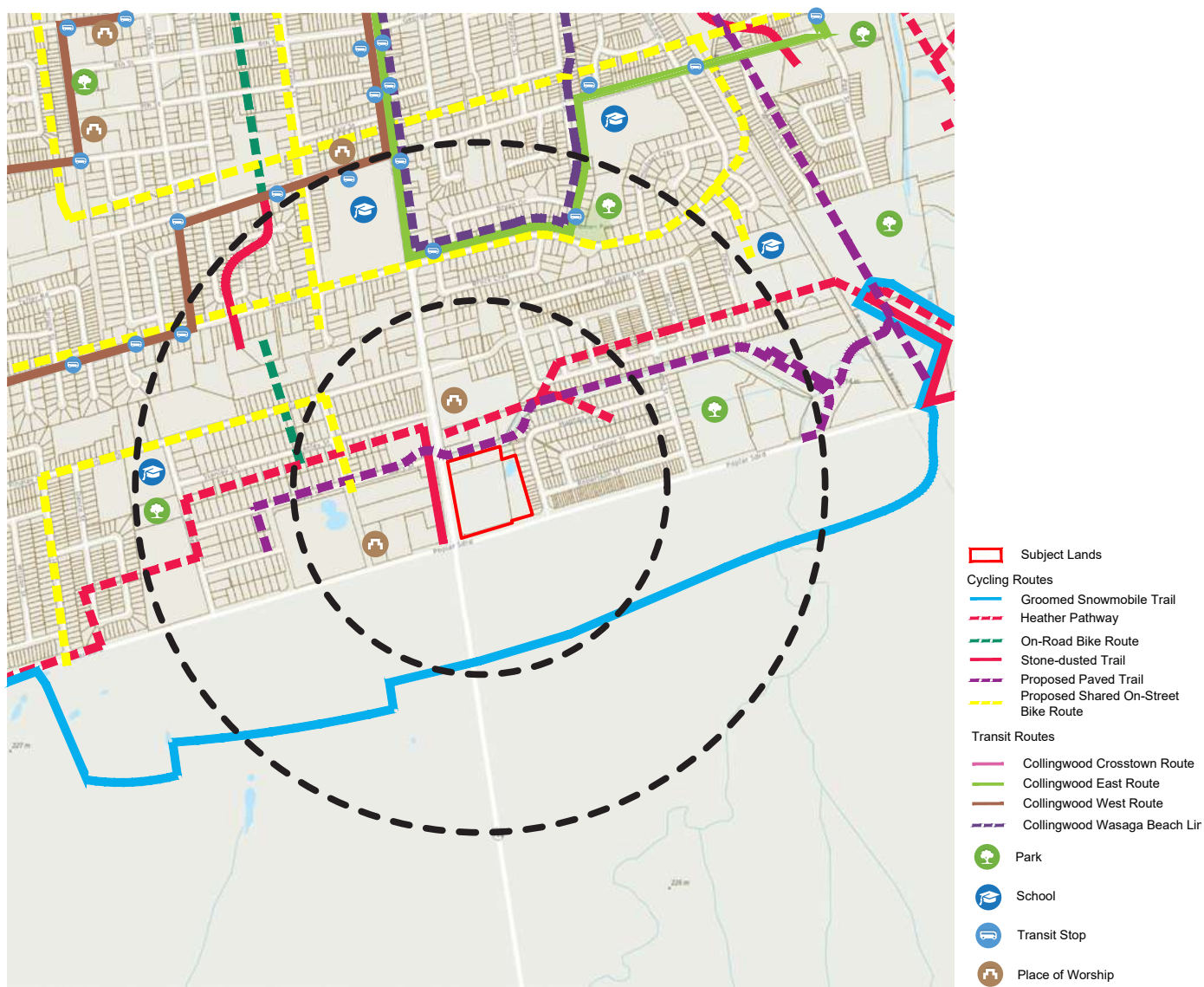


Figure 4.2 Existing Context Map.

PROPOSED CONTEXT

As shown on the Land Use Plan, the subject lands are designated/zoned as “Highway Commercial” along County/Major Arterial Roads. The general character of the area is commercial, surrounded by residential neighbourhoods. The majority of the planning applications are in the vicinity of the subject lands proposed for

residential use. The lands to the west are occupied by a MacEwan Gas Bar and car wash, convenience retail and Tim Horton's, with this development having been approved under the Town's current Urban Design Manual. The Subject lands are surrounded by multiple residential developments outside the Highway Commercial land use designations/zones.

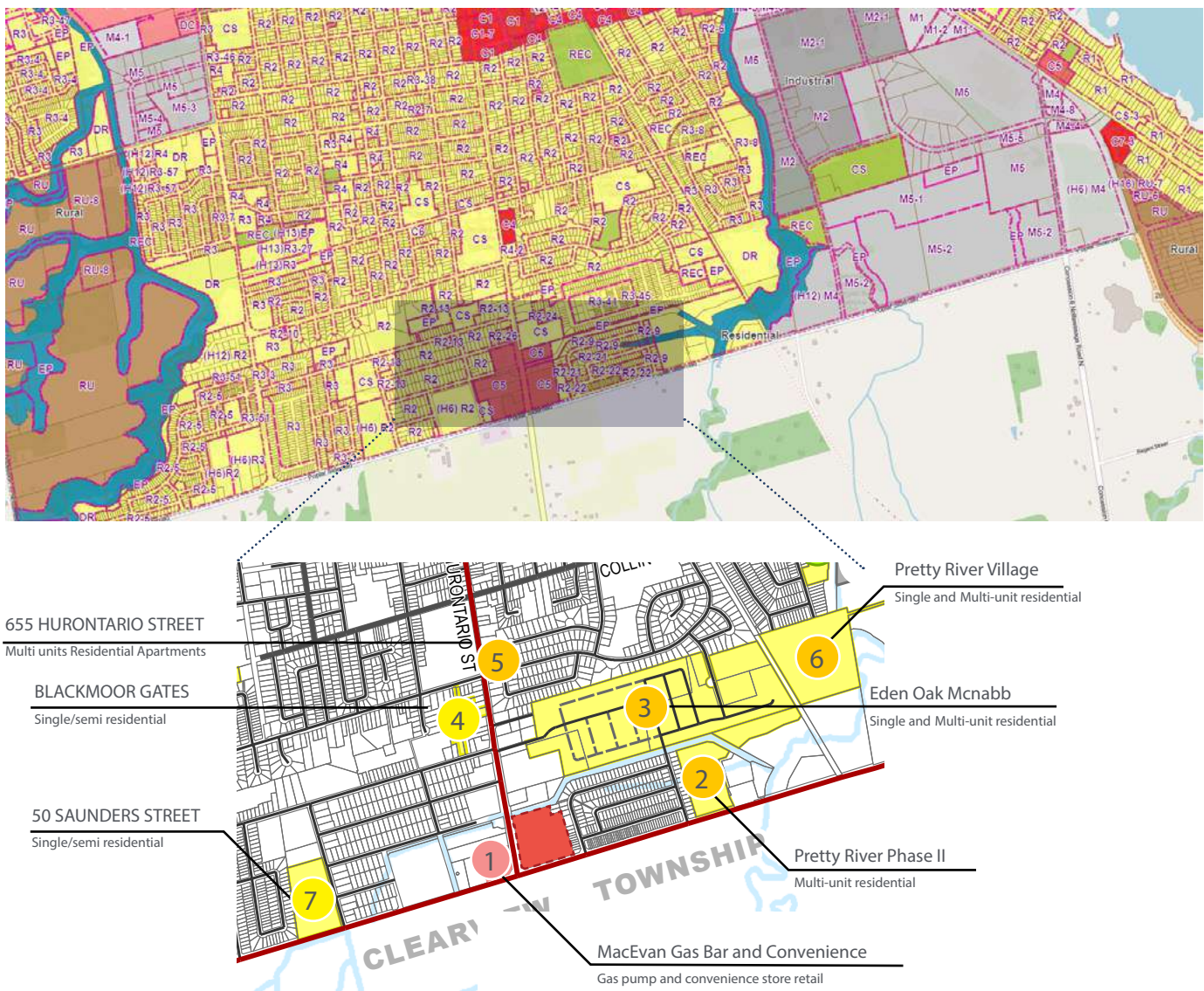


Figure 4.3 Planned Future Context.

SUMMARY

The proposal is compatible with the surrounding built form and land use and complements the future planned context for the area by promoting a mixed-use development at this gateway location that serves residents, surrounding communities, employees and visitors. The proposed buildings on lands designated for highway commercial purposes also augment the area's character as a central gateway to the Regional Service Area/Town of Collingwood. Given that the proposed development represents an increased variety of functions, services and activities, the proposal will complement future developments in the area. The proposed development includes a well-connected pedestrian network and enhanced landscaping that will serve the future planned neighbourhoods and enhance public realm connections to the existing and planned trail system in the vicinity of the subject lands. Overall, the proposal will establish a high-quality development that promotes a prominent, harmonious addition that adheres to complete community principles with opportunities that respond to the evolving needs of the residents.

5.0

THE PROPOSAL

THE PROPOSED DEVELOPMENT

Proposal

The proposal consists of eight buildings for a total of 9,668.44 sq. m. gross floor area ("GFA"), including two drive-through uses - a Starbucks and a McDonald's Restaurant. The development is proposed in three phases.

Phase one includes four single-storey commercial buildings (Buildings 01, 02, 07, and 08), with a GFA of approximately 2868.7 sq. m., featuring a Dollarama retail store and the two aforementioned restaurants with patios and/or drive-through aisles and a building consisting of commercial space, retail and a restaurant with patio. Phase two comprises of two single-storey commercial buildings (Building 05 and 06), including a No Frills grocery store and Shoppers Drugmart with a GFA of approximately 4,731.4 sq. m. Phase three includes the development of a 12-storey mixed-use residential building and a three-story retail building with office spaces above (Buildings 03 and 04), with commercial use on the ground floor, with a GFA of approximately 2,068.34 sq. m.

Each phase will include an associated surface parking area, while the mixed-use building will feature underground parking spaces for residents and visitors.

Vehicular access

The site is given vehicular access from two points located off Hurontario Road in the west, and Poplar Sideroad in the south. The driveway is designed to support convenient

circulation with minimized conflict across the site while providing vehicular access to and from loading/waste removal areas, service stations, delivery spaces and drive-throughs. The proposed driveways are sufficiently wide to accommodate two to three lanes of traffic, including access for fire trucks..

Pedestrian access

Pedestrian sidewalks are proposed around, to and through the site, with access to the site provided at multiple locations, including access directly from the Hamilton Drain Trail and the intersection of Hurontario Street and Poplar Sideroad via Collingwood Parkette. Safe pedestrian access, including sidewalks that are protected by raised curbs within parking areas and internal connections demarcated by decorative concrete paving, are proposed. The sidewalks proposed within the public right-of-ways of Hurontario Street and Poplar Sideroad will provide connectivity to adjacent neighbourhoods and trail system users.

Parking

A total of 274 commercial parking spaces are provided in the form of surface parking spaces within the site. Of these, 13 spaces are dedicated to handicapped parking spaces, 6 spaces for delivery parking and 4 spaces for loading. Furthermore, 297 residential parking spaces are included in the proposal within the underground parking structure. The proposal also contemplates a total of 59

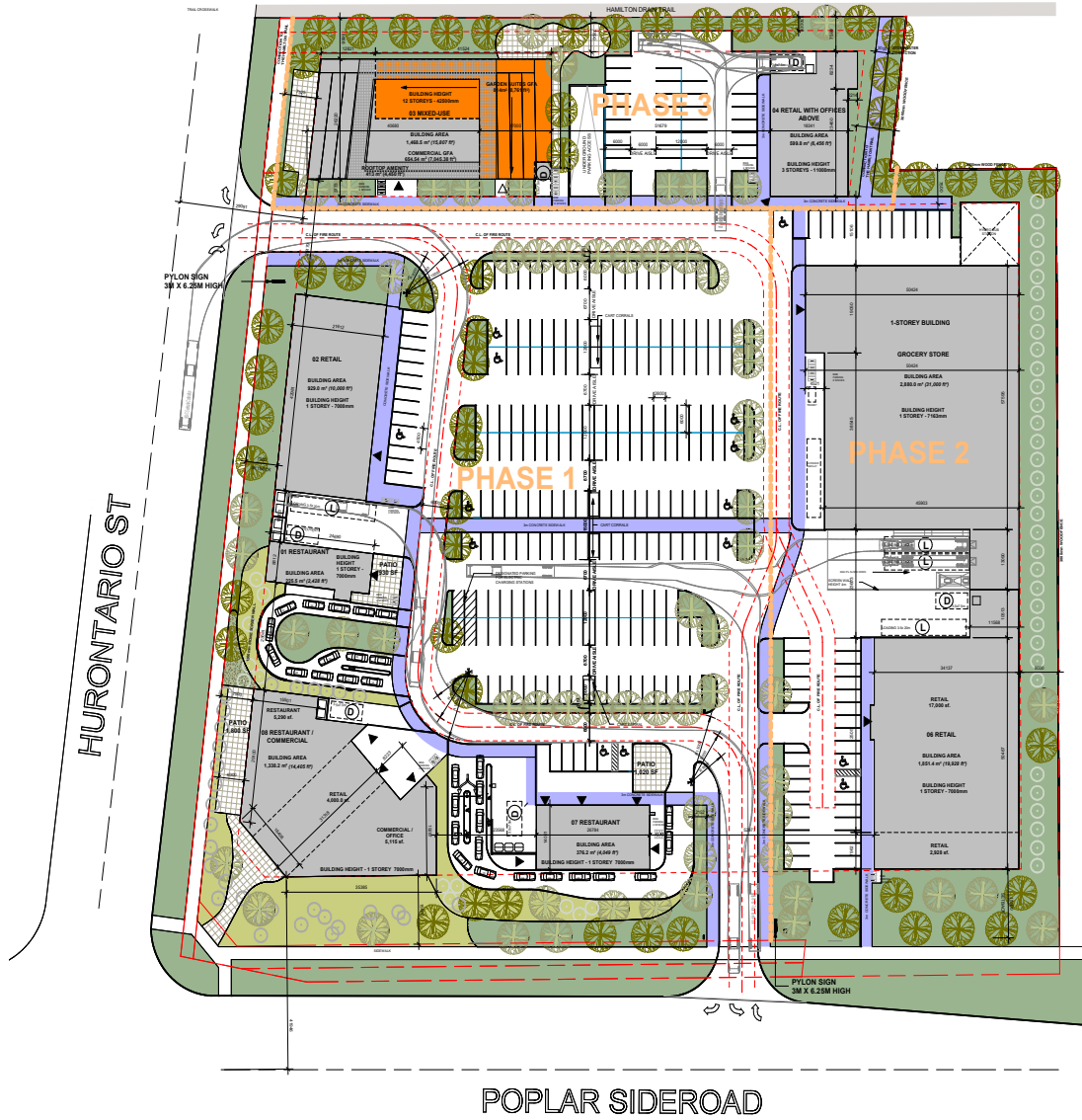


Figure 5.1 Proposed Site Plan

bicycle parking spaces within the site design. The parking spaces and aisles are designed in a way such that they will have a minimum conflict with pedestrian circulation and incorporate landscape islands wherever needed to provide aesthetic relief.

Buildings

The proposed buildings are located close to the surrounding streets at or in close proximity to the minimum setback. Buildings are oriented to accommodate comfortable access for pedestrian and vehicles, space spill-outs (patio areas), and landscape features and

elements. The majority of buildings are massed in a 1 to 2 storey typology and oriented to retain a low rise character and fit in the context. Building footprints are stretched along the surrounding public streets to frame the primary frontages of the site. The active frontages of the buildings are articulated and treated with variations in architectural forms and material to create visually interesting elevations. The buildings design are inspired by the greater Collingwood Area and Niagara Escarpment, using Lakeside and Mountain Recreational vernacular to reflect common themes, material pallet and architectural forms to achieve a high level of integration, and harmony.



Figure 5.2 View looking south-west from Poplar Sideroad

6.0

SITE LAYOUT

UDM SEC. 6

A. Streetscape

1) All development (including subdivisions) shall be arranged to address streets, excepting laneways, as window streets, by lining streets with building front facades, active uses, and public spaces.

2) Buildings shall line streets with commercial/retail, office space, living units, or other active uses; and with functional windows and entrances, rather than parking lots, garages, or blank walls.

3) Interior uses such as seating areas, employee rooms, offices, waiting areas and lobbies, which have the potential for clear windows, should be located along street-facing walls with functional windows and entrances.

5) Buildings shall not be permitted to face their loading or service areas onto existing or planned parks, civic buildings and spaces, or public right-of-way. Based on design merit and the use of screening, alternative configurations may be permitted.

6) Screening/noise attenuation fencing...., They may be permitted along arterial streets based on need and design merit, with particular attention to aesthetic characteristics, integration of landscape and decorative fencing materials, and the overall quality of the streetscape design resulting from their use.

7) Buildings should be oriented parallel to the street centre line and create a 90 degree grid pattern with other adjacent buildings. Groups of buildings or those creating courtyards may be permitted to vary from this requirement to create site differentiation based on design merit.



FOCAL POINT



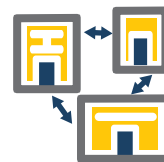
GATEWAY



CHARACTER



FINE GRAIN



CONNECTIVITY



SETBACK

RESPONSE

The highway commercial land use designation generally includes large parcels with a wider commercial scope and site design that gives particular attention to vehicular traffic orientation. The highway commercial land use designation typically involves large parcels designed with a focus on accommodating vehicular traffic. Since the Subject Lands are situated at the southern gateway to the Town, the proposed site layout emphasizes efficient and integrated circulation. The proposal serves the residents within the Site and attracts people from the broader community, establishing the site as a key regional service point.

The proposed site design contemplates a well-connected pedestrian network across the site with direct access to the public sidewalk and planned trail system around the Site. The site has vehicular access from both surrounding roads and drive-through facilities. Loading and service areas are located close to these access points to minimize vehicular conflict with pedestrian movement and space programming.

High-quality landscaping, including planter beds, islands, and strips, will be thoughtfully integrated into the site design to define primary frontages, optimize the functionality of parking areas, and enhance pedestrian movement. These landscaping elements also improve the quality of spill-out spaces across the site. The outer boundaries are landscaped and fenced with soft, decorative elements, metal fences and piers, enhancing the adjacent streetscape and public realm, including sidewalks along Hurontario Street to the west and Poplar Sideroad to the south. The design of Collingwood Parkette

at the southeast corner will incorporate decorative elements and monument signage to establish the area as a central gateway and create a sense of arrival to the Town. Additionally, the vehicular and pedestrian access points off both abutting streets are marked and identified through enhanced soft landscaping and high-quality signage, further emphasizing the site's connectivity and accessibility.

Buildings are thoughtfully oriented and placed within the site to function harmoniously with the public realm, ensuring comfortable and safe pedestrian and vehicular circulation. Buildings 01, 02, 03, 06, 07 and 08 are positioned close to the street, maintaining a continuous edge and framing the streetwall with active facades. Primary and secondary building façades, entrances, patios, and interior uses with the potential for clear windows are strategically oriented to frame and enhance the public realm. The streetwall is further defined by soft landscaping, screen walls, and active uses such as patios and ground-level commercial spaces.

Loading areas, where proposed, are carefully contained within the building envelope or screened by a screen wall to minimize visual impact. The design of the building façades complements the overall site design and functionality, promoting indoor-outdoor space interaction and providing spill-out opportunities that also enhance the safety and vibrancy of the site. The site's programming, including both exterior and interior uses of the buildings, plays a crucial role in successfully integrating buildings, landscaping, and pedestrian and vehicular movement, contributing to a well-coordinated and functional environment.

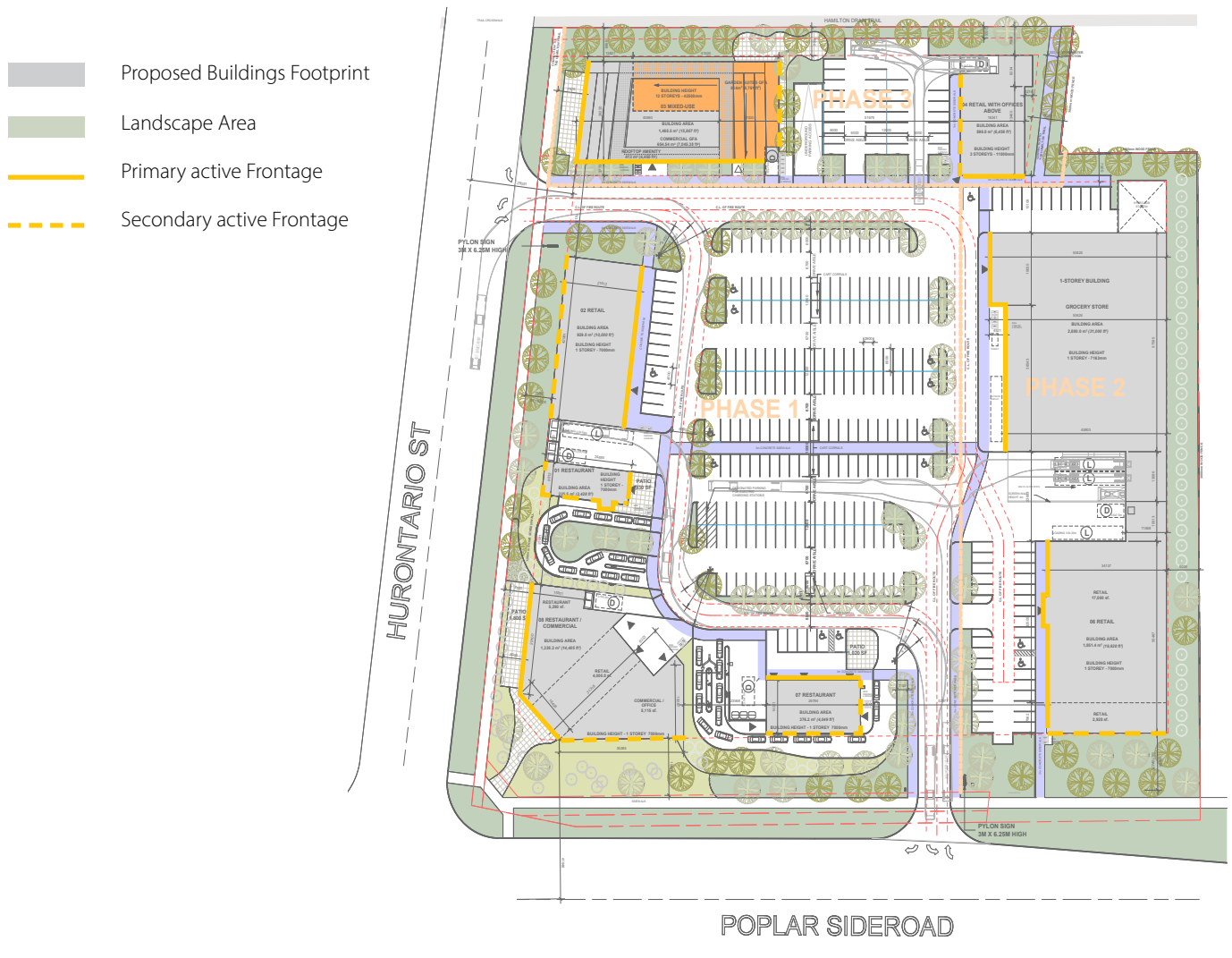


Figure 6.1 Site Plan showing the proposed building location and frontage

B. Parking

1) Parking lots shall be configured, designed and landscaped as well defined areas linked to a particular building with safe and convenient pedestrian ways; more parking stalls than is necessary to comply with the Zoning By-law (for lots with over 120 parking spaces); as opposed to poorly defined asphalted spaces whose only control mechanism is provided by parking stall markings.

3) Large parking lots may be permitted along street frontages as part of an overall plan for large developments such as commercial or retail centres, business and industrial parks, and institutional campuses. These shall:

a. Meet the landscape standards defined in Section 10-Landscaping & Public Spaces; and, b. Shall be located and designed to have minimal impact on the streetscape in terms of disruptions to a continuous streetwall defined by buildings, active uses, complementary landscaping and pedestrian amenities.

5) For commercial and mixed use projects, surface parking may be located beside a building along the primary street frontage provided: a. It does not take up more than 30% of the lot frontage or 30m whichever is less; and, b. It is no closer to the street than the front of the building; and, c. Enhanced parking lot screening as per Section 10-Landscaping & Public Spaces is provided; and, d. The parking area should be bounded by buildings on at least two sides as much as is practical.

8) Each site shall have a single car entrance designated. Additional entrances may be permitted provided: a. It is demonstrated to be a reasonable option due to site conditions or use; and, b. Design features are used to

ensure that potential impacts to pedestrian walkways, and site character are mitigated.

11) Parking areas shall be designed to include direct and safe pedestrian linkages while maintaining pedestrian safety, comfort and access.

12) Paving shall be reduced to the minimum necessary to accomplish site circulation and parking needs with other areas set aside for landscaping, or pedestrian amenity spaces.

RESPONSE

The proposed development features two vehicular access points, strategically located along Hurontario Street and Poplar Sideroad. To facilitate efficient vehicular access and servicing for the McDonald's and Starbucks restaurants, along with their associated drive-through facilities, these two building structures are positioned in the southwest portion of the site, setting back from but having access to Hurontario Street and Poplar Sideroad. The vehicular circulation around these restaurants will be controlled through traffic calming features and ground marking to accommodate safe and comfortable navigation.

The proposed buildings are oriented to frame the proposed sidewalk and accommodate a pedestrian-oriented environment. Pedestrian paths and crosswalks will be marked across the development block to be clearly visible and accessible. The proposed buildings with drive-through facilities are programmed to accommodate a safe and screened drive-through lane with mitigated potential for pedestrian conflict. The proposed queuing lanes are

separated from the traffic aisles and are positioned to accommodate the required length for a short time stop. Vehicles would be able to safely exit the lane and enter traffic aisles with no conflict with pedestrian sidewalks.

The proposed pedestrian sidewalk and the landscape areas are integrated to define building primary entrances and delineate parking areas. For the most part, surface parking is located at the sides or rear of proposed buildings to minimize conflict with the pedestrian environment and reduce visual impacts on streetscape. The shared surface parking lot is located at the centre of the site for easy access. A central pedestrian walkway is provided to allow for safe, direct pedestrian movement across the parking field.

As mentioned previously, underground parking is proposed for Building 03 and is located to the east of the building, directly accessible from the internal driveway.



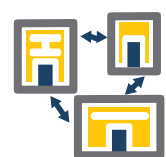
CIRCULATION



FINE GRAIN



STREETWALL



CONNECTIVITY

Figure 6.2 Proposed surface parking area



View from Polar Sideroad, South Entrance



View from Hurontario Street, West Entrance

Figure 6.3 Renders of the proposed development illustrating building orientation and vehicular access and pedestrian circulation in relation to public realm and surrounding streets

D. Arterial Corridor

1) Along all arterial road and highway corridors the minimum building facade height shall be defined by a building mass of two storeys or 7m (not including roof) to provide a more human scaled relationship between the proportions of the street width and building heights.

E. Relationship of Uses

1) To avoid unnecessary segregation of uses, developments shall not rely on blank walls and fencing as the primary means to avoid possible negative impacts on neighbours.

2) A combination of landscaping; compatible building massing, façade design and detailing; and, use of public spaces shall be the methods used to reduce negative impacts and transition between developments and neighbouring uses.

F. Site Character

1) To help promote differentiation and character, developments over 1,500 square metres in floor area shall incorporate at least two of the following where appropriate:

- a. Decorative walkway patterning and/or materials that are different from the public sidewalk;
- b. Public art feature(s);
- c. Decorative wall-mounted or freestanding light fixtures for pedestrian ways;
- d. Decorative metal fencing;
- e. Roll-up or large operable windows and doors adjacent to outdoor seating areas;
- f. Landscaped trellises or other decorative element that incorporate landscaping near the main building entry;
- g. Decorative entry features;
- h. Decorative bike shelter; or,
- i. Decorative street furniture/functional elements.

RESPONSE

The proposed development is located at the southeast corner of the central gateway to the Town of Collingwood and along two major arterial corridors. The overall character of the area represents a diverse mix of retail, office, and residential uses that serve residents and visitors. The development site is bordered by low-rise residential properties to the east, with public streets—Huronario Street and Poplar Sideroad—defining its western and southern edges. To the north, the site is adjacent to the Hamilton Drain Trail.

The proposed development accommodates the transition to the neighbouring residential uses through landscaping elements, including soft features and planting materials. Decorative metal fences and piers are proposed at the west and south edges of the Site along the street interface to create a sense of place and definition. Buildings are oriented to frame the public realm in keeping with the guidelines and in contrast to the existing commercial complex to the west (MacEwan Gas Bar and Convenience) across Huronario Street.

The site is integrated into a well-connected pedestrian network, with sidewalks linking building entrances to the existing pathways along Huronario Street, Poplar Sideroad, and the Hamilton Drain Trail to the north. These sidewalks will be clearly marked and illuminated to ensure comfortable and safe pedestrian navigation throughout the site. The pavement will be constructed from high-quality materials, featuring decorative patterns that complement the architectural design and materials of the buildings, contributing to a cohesive and unique character across the site. Decorative lighting will be incorporated

wherever possible, enhancing both the aesthetic appeal and the safety of the pedestrian environment.

Building entrances and active spaces such as patios are oriented inward to provide direct access from parking spaces, avoid conflict with vehicular traffic, and address safety concerns. Opportunities for direct pedestrian connections from the intersection are protected for in the design, and would be subject to coordination with tenants and their use of the unit space. The building design incorporates articulation, glazing, and large and clean windows along public frontages to animate the public realm and enhance the pedestrian realm. Landscape areas will incorporate enhanced softscape features, large trees, and decorative screening, such as metal fences and piers, to improve a visually appealing site and assist with the transition to the public realm and neighbouring uses. The site design incorporates landscaping along the building's frontages, patios, near entrances, and within the parking lot to help promote the Site's character and visual interest. The Site entrances will be enhanced with landscape features and signages.



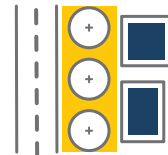
CHARACTER



ANIMATION



FACADE



LANDSCAPE BUFFER



Figure 6.4 View towards Shoppers showing parking, active frontage, sidewalks and landscape buffer

G. Patio & Dining Area

1) Patios and dining areas shall be designed to create a compatible and complementary relationship with adjacent streetscapes, building architecture and uses. This includes size and proportion to neighbouring site elements; materials; and landscape design.

3) Changes in elevation and/or, raised planting beds and planters, may be used to define the edge of a patio or outdoor dining area, provided barrier-free access is maintained and the design is generally fitting with the streetscape.

J. Open Area

1) Open areas shall be grouped into useable, prominent landscaped areas, rather than equally distributed into configurations of low impact such as at building or site peripheries, or at locations not visible from public areas.

2) Site layout shall facilitate physical and visual access to adjacent or nearby parks, trails, open spaces, water courses, waterfront, and similar public spaces to reinforce their public profile and safety.

L. Building Service uses

1) Specifically designated areas shall be provided for uses such as service entrances, delivery and sorting, temporary storage, garbage and recycling, outdoor storage, outdoor work areas, and other similar uses. These shall be: a. Located behind buildings; b. Appropriately sized for the intended use; and, c. Screened from public areas and residential uses to reduce visual, or sound impacts on adjacent uses, as per UDM Section 10-Landscaping & Public Spaces.

2) Truck maneuvering, circulation, and queuing lanes shall be signed, and marked accordingly on the pavement.

4) Loading and delivery areas should not be located in a required setback area.

5) Uses shall provide sufficient on site truck queuing areas as necessary for the expected numbers of trucks. These shall be located behind buildings and screened as practical.

RESPONSE

As mentioned previously, the ground floor of the buildings is programmed in relation to the public realm, while the outdoor spaces, including spill-out areas, are designed to complement the surrounding streetscape by animating the pedestrian environment and creating a visually appealing streetwall. Wherever patios are proposed, these areas are oriented to frame the public realm, such as the patio areas located in the front yard of Buildings 01 and 07.

Patios will be designed to integrate into the buildings' overall architectural design and will use high-quality and durable materials, including brick, natural stone-coloured veneers, and other compatible materials to enhance visual appearance along the public realm. The landscape areas that frame patios will be ramped up at the edges, and soft features will be used to delineate these outdoor areas.

The pedestrian sidewalk layout across the site is designed to provide mid-block connections while connecting to

the surrounding public streets and Hamilton Drain Trail to the north. The landscaping along the trail and at the connection points will be augmented to increase the quality of linkage and enhance the pedestrian experience.

Buildings are provided with loading and service areas, which are strategically located along the proposed driveways for efficient, safe, and fluent access. These areas are situated on the inferior side of the buildings to minimize the physical conflict with indoor and outdoor spaces and reduce the visual impact on the public realm. These areas are also located away from the residential areas to the east and northeast (the high-rise mixed-use building) to minimize potential impacts.

The Proposed driveways will be signed and marked clearly to accommodate safe navigation. Where loading areas are contained within the building envelope, such as the grocery store, sufficient clearance height is provided.



Figure 6.5 Proposed Patio Area



- Patio Area
- Delivery Spaces
- Loading Spaces



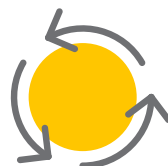
Figure 6.6 Site Plan showing patio areas, delivery spaces, and loading spaces



PEDESTRIAN-ORIENTED



WAYFINDING



CIRCULATION



ACCESSIBILITY

N. Utilities

1) Switch boxes, transformers, electrical and gas meters, ground mounted air conditioning units, and other aboveground or building-mounted mechanical equipment and utility elements (including antennae or satellite dishes), shall be located away from development entrances, street intersections, public spaces, or the front façades of buildings; due consideration shall be given to locating these where they can provide the necessary service with a balance between economical and aesthetic parameters.

1) The items listed in 1. above should be located and arranged so as to appear visually integrated within their particular context by locating them: a. At the edges of landscape areas and open spaces; b. Back from pedestrian walkways; c. At lot lines between properties when this can reduce their visual prominence; d. At the edges of buildings; and, e. In under-tree locations where practical;

O. Lighting

1) A cohesive light plan shall be developed where the quality of light produced, and type of light sources used on the exterior of buildings, signs, parking areas, pedestrian walkways, and other areas of a site, are compatible with, and appropriate to the overall design and use of the site.

3) Lighting shall be dark sky compliant, providing full cut off, or cut off, light fixtures; and be designed and located to prevent light from leaving the site.

4) All building entrances, and pedestrian areas shall be lit with pedestrian-scale lighting. Trails may be exempt from this requirement.

5) Light fixtures and associated hardware attached to the exterior of buildings shall be architecturally compatible with the style, materials, colours and details of the building.

P. Trash and Recycling

1) Trash and recycling storage and servicing areas should be inside of, or integrated into the building design.

2) Outdoor trash and recycling enclosures shall not be visible from the main street frontage.

3) Trash and recycling enclosure shall be located such that they are not visually prominent from activity nodes and public areas such as outdoor amenity spaces, gathering areas, parks, and trail entrances.

RESPONSE

Above-ground utility equipment will be located away from development entrances and site access points and will be arranged to integrate into the overall site and building design. Wherever possible, these utilities will be buffered with landscape elements, or concealed along building edges to avoid visual impact and physical conflict.

The proposed development will incorporate a complimentary lighting design along with the site and building design to spot the parking area, pedestrian sidewalk, and building entrances, ensuring sufficient luminosity for clear sight lines and further promoting safety for users. Lighting will be dark sky-friendly with the downcasting feature to eliminate impact on the neighbouring property and prevent light pollution across the site and at the getaway location. Light fixtures

across the site will incorporate different styles and design standards to accommodate appropriate and integrated lighting. For instance, along building frontages and corners, light fixtures will be scaled for a pedestrian and compatible with the architectural style of the building.

Waste and Recycling storage will be contained within the building envelope and integrated into the overall architectural form and material. Wherever outdoor recycling enclosures are proposed, these are located on the inferior sides of the buildings, away from outdoor activity areas and public view.



Figure 6.7 Photo examples of gateway, street, and building Signage

7.0 BUILDINGS

UDM SEC. 7

A. Building Placement and Orientation

1) Buildings shall be designed for an urban context directing their primary facades to the street to create window streets. Rather than being simply pushed closer together, as in many suburban developments, buildings must be designed for close siting which facilitates street activity and active transportation, with views directed to the street and public spaces not toward neighbouring sites.

2) Buildings shall be placed at the front setback line,...

5) Main entrances shall be visible from, and directly accessible from, the street.

9) Buildings, entrances, windows and activities shall be oriented to relate to the street and public spaces, to establish a coherent development pattern that is people-oriented, creates window streets and defines the streetwall.

10) Building design and placement should address sunlight penetration; ventilation; protection from prevailing winds; public views from adjoining structures; and, public spaces so that they maintain their utility.

C. Corner Site

1) Building on corner sites shall be located at the setback lines of each street frontage.

D. Building Grouping

1) Multiple buildings in a single project shall demonstrate a functional relationship with one another and the public spaces of the development by: a. Taking into account microclimate including: i. Daylight and sunlight exposure; and, ii. Wind and temperature; and, iii. Minimizing the impacts of shadows on outdoor spaces for people; b. Minimizing the impacts of undesirable shadows on adjacent buildings; c. Creating compatible relationships and access between interior spaces, exterior spaces, and entrances of different buildings; d. Arranging buildings to afford both passive and active solar access potential for adjacent buildings; and, e. Creating open areas with comfortably scaled relationships between

3) Within multiple building developments the architecture shall be complementary in style, form, colour, material and roof line.

4) Visual and physical pedestrian links shall be established between buildings that are not located along a street. These links may be accomplished through the use of arcades, trellises or other similar open structures, or landscape features.

5) In multiple-unit residential developments with multiple buildings, indoor privacy between living units should be facilitated with consideration in the design of the entire complex and buildings to specifically address views

into residential units from other units within the same development. Techniques such as varied separations, or changes in building orientation and floor plan of units should be considered.

E. Context

- 1)** Buildings and additions shall be designed to: a. Improve the overall streetscape, and public realm and; b. Generally relate in scale, materials and design features to the surrounding buildings; and,
- 2)** Building design shall respond to the local climate with features such as pedestrian weather protection, overhangs for sloped roofs, facilitating access to direct sunlight, and choice of façade materials.
- 3)** When compared to neighbouring developments and buildings, the use of significant contrast in building design, and materials can be used to: a. Make a project unique for the purpose of announcing the importance of a use; b. Provide a focal point within a district; c. Highlight a priority site; and, d. To create accent along a streetscape.

F. Height and Mass

- 1)** Buildings over 10m in height (offacade) shall incorporate any combination of the following design elements to reduce their apparant mass along the street:
 - b. Setting the building back so that it does not project into a 45 degree angular plane from the street centre line and integrating outdoor amenity space and/or landscaping in the increased setback.
- 2)** Buildings with more than three stories, or 10m greater in height of façade), than adjacent uses shall incorporate any combination of the following design elements to reduce their appearant mass along side yard property lines:
 - b. Incorporating specific landscaping to screen views; and/or,
 - c. Setting the building, or portions of the building, back so that it does not shared property line.
 - d. Other alternative design solutions that effectively reduce the visual impact of these larger buildings along the street (or on adjacent uses) and create building massings that are more human-scaled or compatable with adjacent buildings.

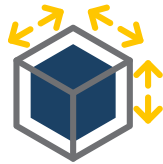
RESPONSE

The proposal involves the development of a large parcel, featuring a group of buildings that are configured and designed to ensure compatibility with the surrounding context while establishing physical and visual interaction between indoor and outdoor spaces. The buildings are connected through a well-knit pedestrian network across the site, linking primary entrances both within the site and to the public sidewalks. The space programming around

the buildings is designed to support the ground-floor functions of the buildings in relation to each other, as well as the overall site design and outdoor activities. Buildings are located close to the surrounding arterial roads are with some programmed with active uses fronting the public realm to activate and animate these areas.



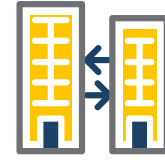
BUILT FORM



MASSING



SETBACK



COMPATIBILITY

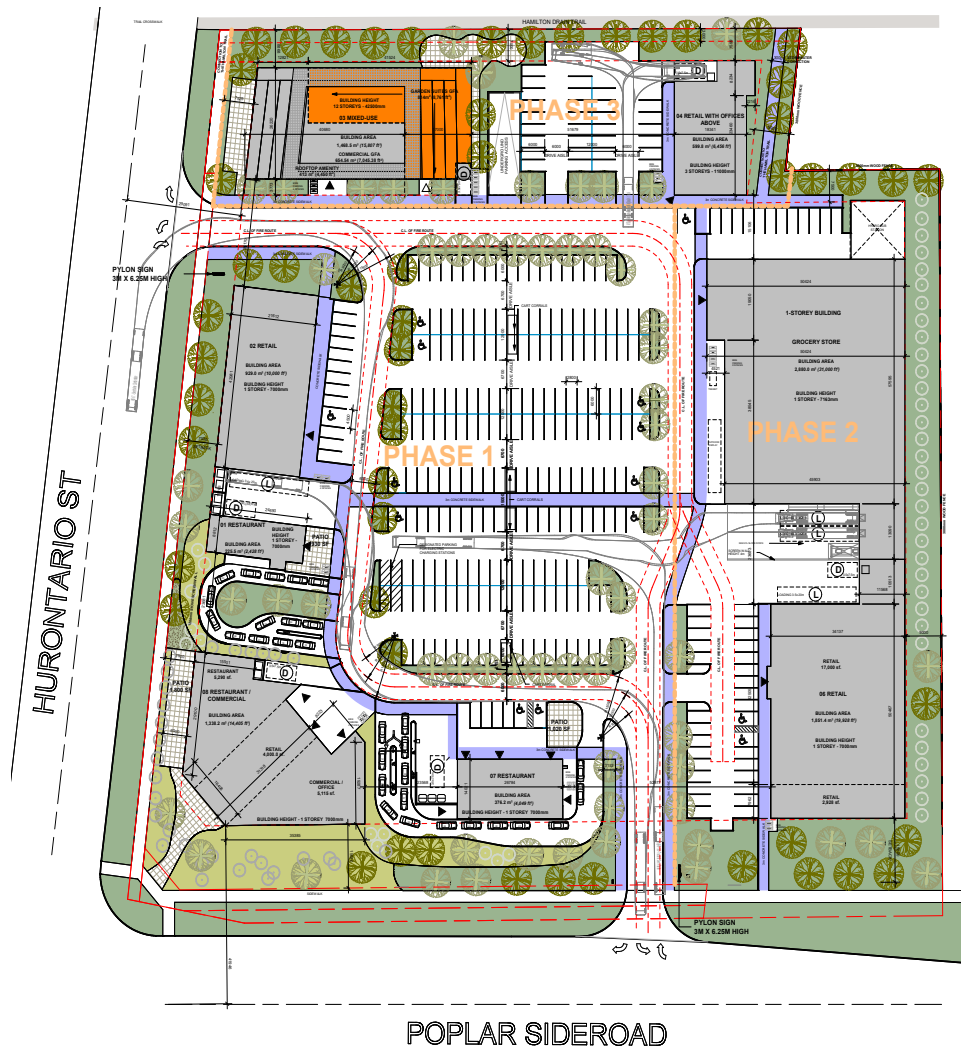


Figure 7.1 Site Plan showing building orientation and site layout

For instance, buildings 01, 02, 03, 07, and 08 are positioned near the street to maintain a continuous edge and create engaging streetwalls. Additionally, primary building façades, entrances, patios, and interior spaces with clear windows are oriented and arranged to enhance the streetwall. Within the site, the location of primary entrances aligns with pedestrian and vehicular circulation to facilitate efficient site navigation.

Given that the Subject Lands are adjacent to existing and proposed residential neighbourhoods to the east, the proposed buildings orient their primary façades toward surrounding streets or inward to the Site, eliminating impact on the residential area while animating the street interfaces and providing convenient access.

To establish a coherent development within the context, the buildings are programmed with active spaces and visual connections that either engage with the public

realm or the pedestrian environment within the site. This combined indoor/outdoor space programming fosters continuous spatial interaction, resulting in a dynamic, safe, and comfortable pedestrian environment. The building design and articulation will incorporate a common theme to create a complementary and cohesive development that reflects the unique character of the site, integrates with the surrounding context, and enhances the site's identity as a central gateway.

The placement and orientation of the buildings will enhance site design excellence, provide spill-out opportunities, and create a harmonious design that, combined with enhanced landscaping, will improve the overall quality and safety of the streetscape and public realm. The site programming, including indoor and outdoor spaces, also contributes to the successful integration of uses, buildings, landscaping, and pedestrian and vehicular movement.

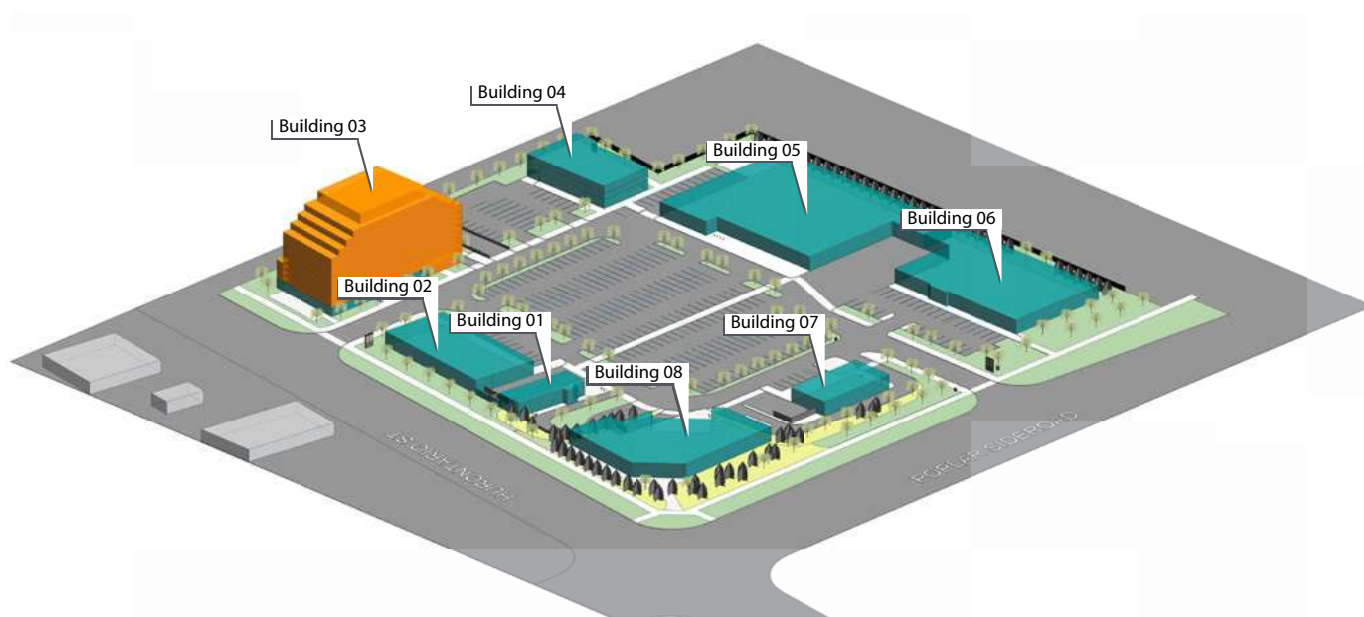


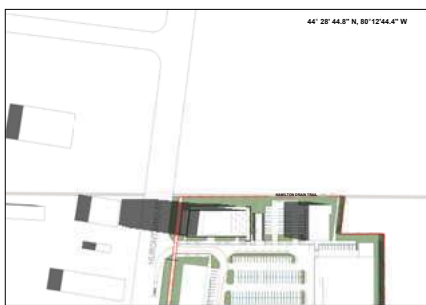
Figure 7.2 Site 3D view- southwest

As discussed previously, buildings are predominantly massed 1-storey low-rise typology, while buildings 03 and 04 are proposed with a greater height at 12 and 3-storey, respectively, to accommodate residential, commercial, and office use while providing landmark and wayfinding structures at the gateway location. Building footprints are stretched along the surrounding public streets to frame the primary frontages of the site.

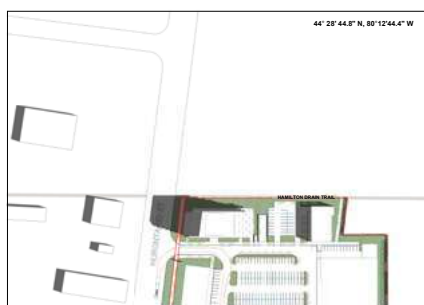
Buildings 03 at 12 storeys and 04 at 3 storeys are oriented and articulated to ensure appropriate transition from the neighbourhoods to the east and the opposite side of the Hurontario Street. This is achieved through setbacks and stepbacks that ensure the built form remains below a 45 degree angular plane, limiting shadows, overlook and loss of privacy to neighbourhoods, and allowing for compatibility with future mixed uses to the north. The massing is designed on a human scale through a

29.09-metre setback from the centre line of Hurontario Street, and a progressive step back starting from level 10th, again ensuring the building envelope respects the 45-degree angular plane.

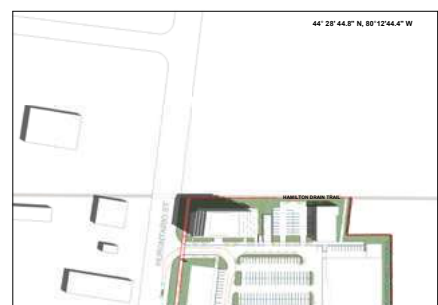
A Sun-Shadow study for building 03 and 04 has been prepared by Richard Ziegler Architect Inc. in support of the proposed Applications; tests completed at hourly increments for June 21st, September 21st, and December 21st in accordance with the requirements for the development. The Study demonstrates that an proposed shadows move quickly across adjacent properties with shadow only being drawn on neighbourhoods to the east after 6:12pm during the spring and fall equinoxes. As a result, the neighbourhoods have unrestricted solar access for the vast majority of the day, deeming the proposed shadows as within an acceptable range.



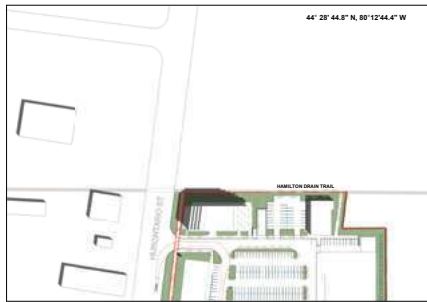
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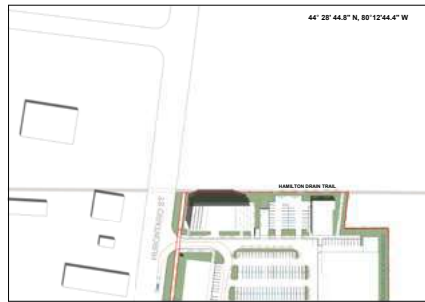
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JUNE 21, 9:20 AM



JUNE 21, 1020 AM



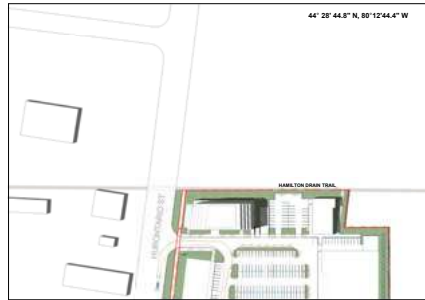
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JUNE 21, 1220 PM



JUNE 21, 120 PM



JUNE 21, 220 PM



JUNE 21, 320 PM



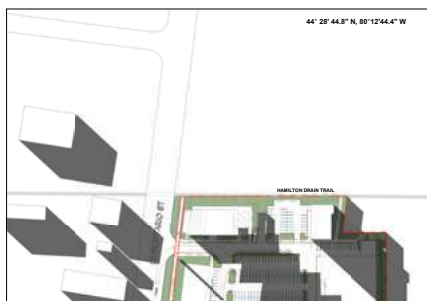
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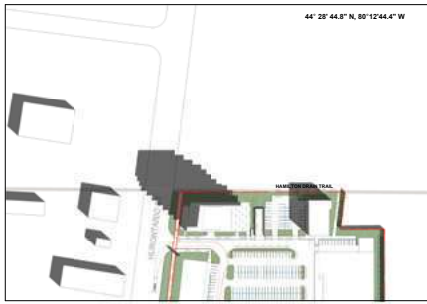


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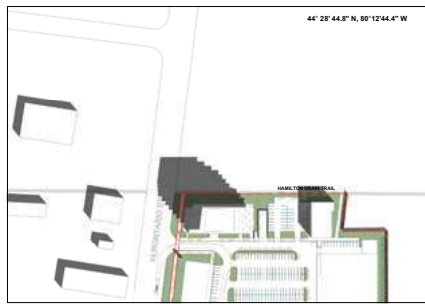


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Figure 7.3 Shadow Study- June 21st



SEPTEMBER 21, 835 AM



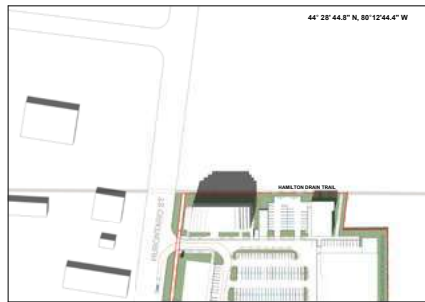
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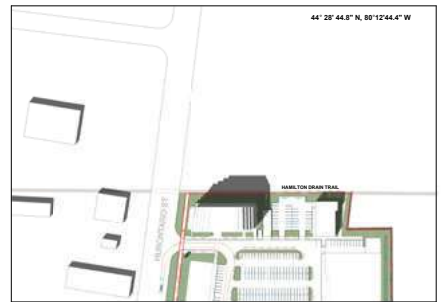
SEPTEMBER 21, 1012 AM



SEPTEMBER 21, 1112 AM



SEPTEMBER 21, 1212 PM



SEPTEMBER 21, 112 PM



SEPTEMBER 21, 212 PM



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SEPTEMBER 21, 412 PM



SEPTEMBER 21, 512 PM

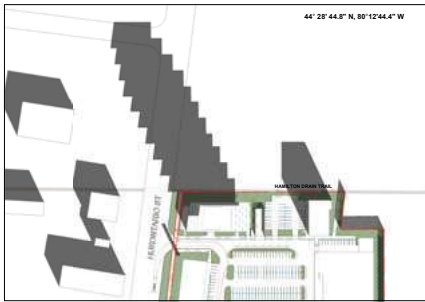


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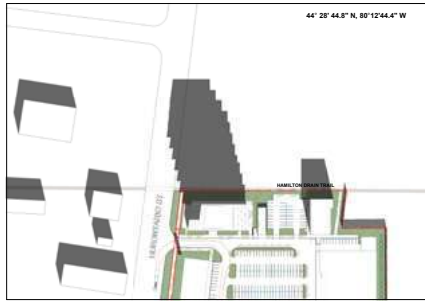


SEPTEMBER 21, 712 PM

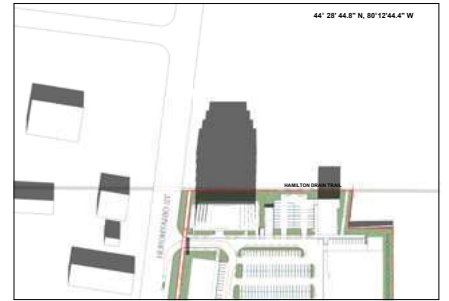
Figure 7.4 Shadow Study- September 21st



DECEMBER 21, 919 AM



DECEMBER 21, 1017 AM



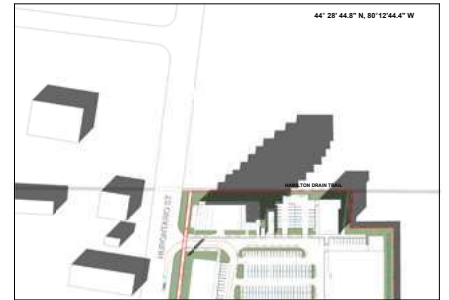
DECEMBER 21, 1117 AM



DECEMBER 21, 1217 PM



DECEMBER 21, 117 PM



DECEMBER 21, 217 PM



DECEMBER 21, 315 PM

Figure 7.5 Shadow Study- December 21st

G. Building Facade

1) In addition to the specific requirements of the UDM, the design of non-residential buildings should meet one of the two architectural themes identified for Collingwood listed below: a. Traditional Small Town Heritage Style; and, b. Lakeshore and Mountainside Recreation.

2) To provide appropriate contextual fit with existing development or neighbourhoods, or to better meet the characteristics of the architectural themes identified in 1. above, projects may be required to amend their designs with characteristics such as the following: a. Brick or masonry detailing; b. Building facades incorporating traditional architectural detailing of windows and doors; c. Building architecture incorporating distinctive elements such as pitched roofs, timber beams, cedar roofs, masonry, or wood siding; d. Building massing that reflect those of heritage buildings within the community; e. Specific fenestration details.

3) Facades shall provide visual interest through a combination of windows, entrances, architectural details, projections, and recesses. These elements shall create a consistent rhythm, and express a hierarchy of entrances, and identify individual businesses where applicable.

4) Vertical and horizontal façades plane breaks will be incorporated to provide visual interest for facades greater than 30m in length to break up the apparent mass of the building and to add visual interest.

6) Weather protection shall be provided along building facades containing retail/ commercial uses, as well as over building entrances.

11) The design of buildings shall be unique to the site and project, and not corporate, chain, or franchise. Corporate, chain, or franchise architecture may be permitted if the design (or amended design) is determined by the Town to meet the architectural characteristics of either of the identified architectural styles described in the UDM, or another complementary design/style that is determined by the Town to be suitable to the site and project in question. The only franchise-identifying features should be minor facade details, the company logo, and signage.

13) Large commercial/retail buildings or shopping centers with multiple tenancies, and/or multiple uses, shall be arranged and designed to provide storefronts and entrances for each of these along the primary facades of the building as determined appropriate and practical by the Town.

H. Articulation

2. Building façades shall demonstrate a unified appearance with complementary materials and colours. The use of oversized, or out-of-proportion, design elements and building features may not be permitted.

I. Entrances

1. Main/primary entrances shall be distinguishable from other façade elements and entrances: a. In terms of architectural design, decorative treatments, and/or building massing; and shall be, b. Oriented to, and visible from, the street.

3. Ground floor entrances facing the street frontage or public spaces shall be functional and provide access into the building. Emergency exits are exempt.

5. Every building should provide at least one entrance that does not require passage through a parking lot from the front sidewalk to gain access.

J. Fenestration

1. Windows shall be architecturally compatible with building style, and materials.

3. Windows and doors shall be proportioned to the size of wall in which they appear and, sufficient wall area and/or architectural features between windows shall be provided to set them apart from each other. Wide mullions may be permitted based on design merit.

8. Transom windows are encouraged in commercial and mixed-use projects if appropriate to the architectural style of the building.

M. Retail Facades

1. Storefronts shall be the most transparent part of a façade. These shall have the following common elements: a. Base; b. Storefront display windows; and, c. Canopy/sign band.

2. Ground floor commercial/ retail, service, office, and restaurant uses shall have large display windows, framed by the surrounding wall.

3. Storefronts shall be defined in a repeated rhythm along the facade to maintain continuity and pedestrian interest.

4. Storefronts shall be integrally designed to be compatible with the entire façade character.

5. Storefront windows should occupy the entire height between the base and the canopy area.

6. Storefronts within the same building shall use similarly sized and shaped windows and doors.

7. Fully openable display windows are encouraged for restaurants, cafés and uses adjacent to outdoor spaces, dining areas, and activity nodes. These may be required if determined appropriate by the Town based on overall design merit.

8. Façades with multiple storefronts should have a generally consistent storefront design and materials. This includes: a. The size and type of display windows; b. Doorway locations; c. The design of transom windows; and, d. Storefront base height and materials. Buildings with more than four storefronts may vary elements, but should maintain an overall consistent façade design.

O. Facade Material

1. Building materials and colour schemes shall be consistent with the chosen architectural style.

4. Simulated materials may be used if determined to have an authentic appearance.

P. Colour

1. Architectural colour palletes shall be selected to complement: a. First, the local historic context of Collingwood's architecture or one of the architectural themes defined by the UDM; b. Second, the context of the immediate neighbourhood of adjacent and surrounding developments on the same block or street section; and, c. Third the colour pallette chosen to define the unique architectural style/ character of the building o development.

2. Colour pallets and patterns associated with a particular corporate identity/image/ branding shall be designed as highlights, similar to signage, and not as the defining characteristic of the building/ development in question. They may be included provided that: a. They are not the predominant colour; b. They do not define a particular building mass in such a way that it appears as a colour field upon which signage is placed or disrupts the other architectural features, or patterns of the façade in such a way that is uncharacteristic of the overall façade design; and, c. The overall color scheme meets 1. above.

RESPONSE

The proposed building design will provide a common theme in terms of architectural design and material selection to achieve a high degree of integration across the development, in keeping with the spirit of the Lakeside and Mountainside Recreation vernacular. Additionally, the buildings' façade design will incorporate variations in form and material to create visual interest and to reflect the prominent character of the area as a highway commercial site along the edges at the southerly gateway to the Town of Collingwood. Architectural colours and materials will be selected to complement the immediate context and existing and proposed commercial developments.

To better achieve a unified appearance, buildings are articulated with comparable architectural forms, fenestration, detailing, windows and doors. Buildings massing and articulation stay in coordination in terms of overall built form, façade articulation, window size and type. To fit into the small-town style, buildings' design will incorporate materials with brick, wood and stone texture/ theme with an authentic appearance.

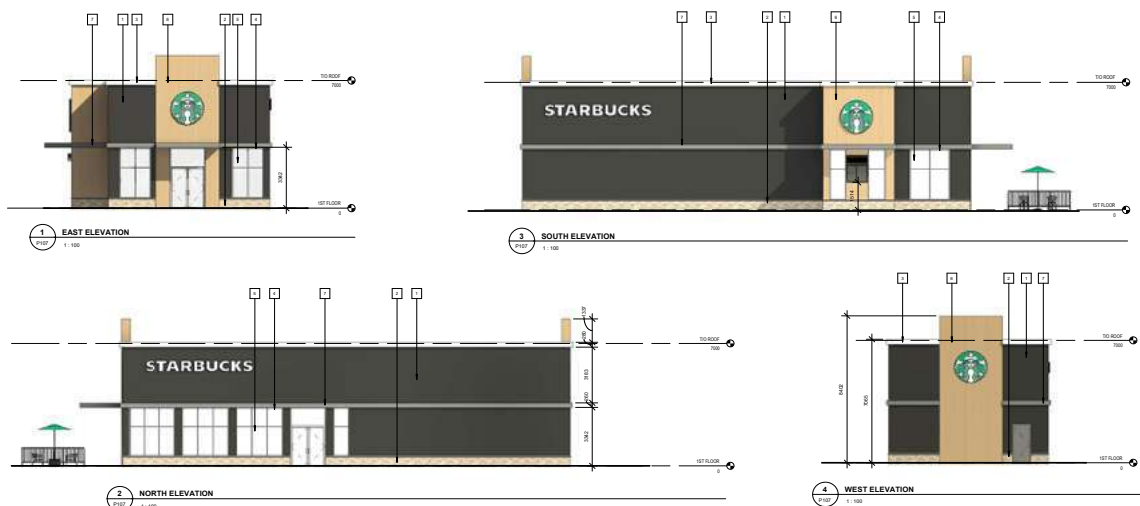


Figure 7.6 Building 01 Elevation



Figure 7.7 Building 02 Elevation

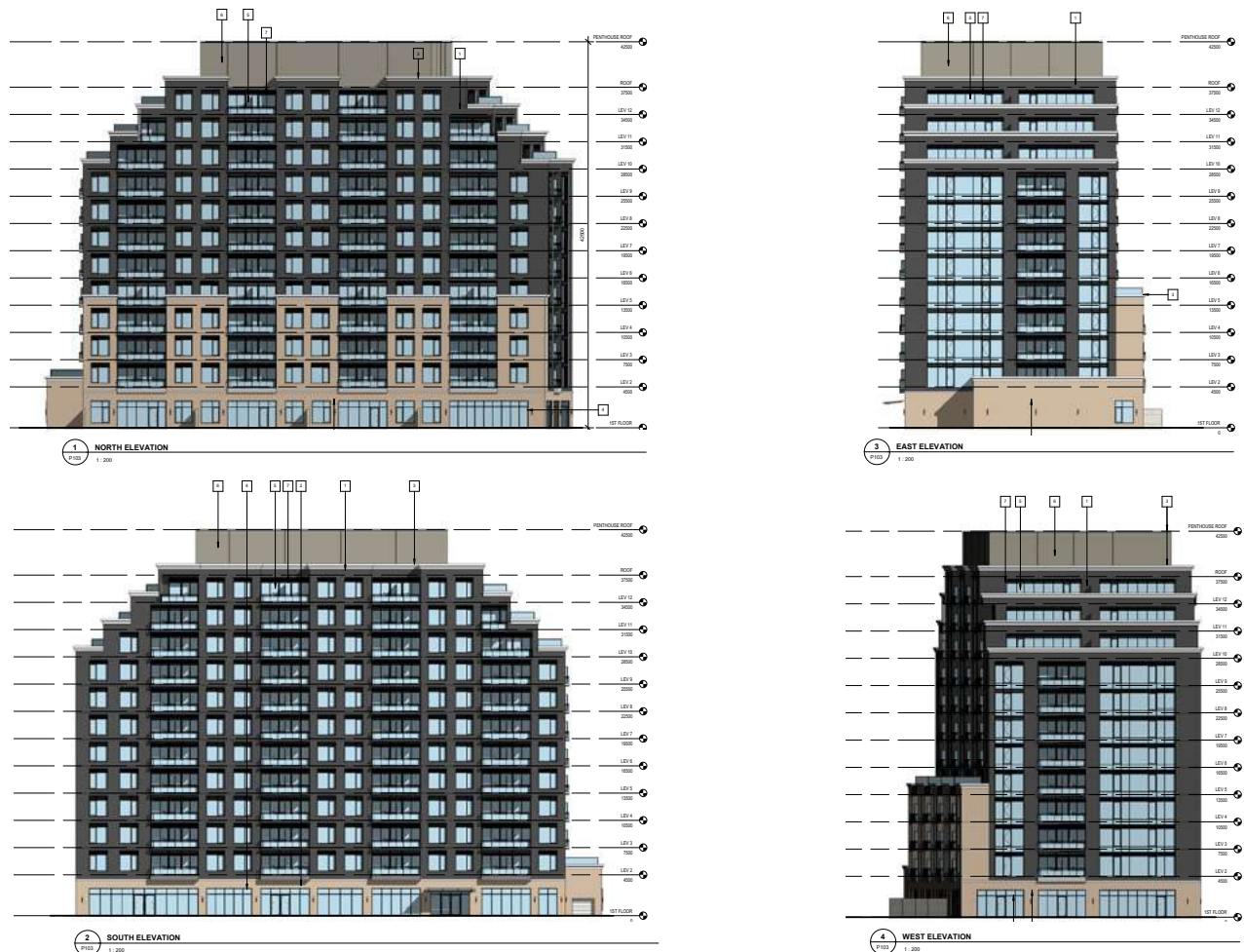


Figure 7.8 Building 03 Elevation

Additionally, buildings' façades across the Site incorporate complementary materials and colours to integrate into the common architectural language and overall image of the gateway around the edges, with the anchor building 06 standing out internal to the site. The proposed architectural design attempts to limit franchise-identifying features, when applicable, to minor façade detailing, logo, and signage to stay unique to the site character. Materials and colour palettes associated with related corporations will be used as highlights and not as predominant characteristics that define the built form and the overall architectural design of the buildings.

Building 03 will stand out due to its distinctive materiality and colour, further promoting its role as a landmark at the gateway location, while maintaining compatibility with

the overall design and surrounding context. The building will feature high-quality materials, including red brick, limestone, glass, and an aluminum window wall system, ensuring a cohesive and attractive architectural expression throughout the site.

For façades of greater length, such as the proposed Building 08, the built-form design incorporates variation in form and planes, along with material changes and architectural details, to break up the building mass and create visually engaging and active elevations along the primary frontages.

The building façades demonstrate a high level of fenestration and glazing along the primary frontages, incorporating variation in form, plane breaks, architectural



Figure 7.9 Building 08 Elevation

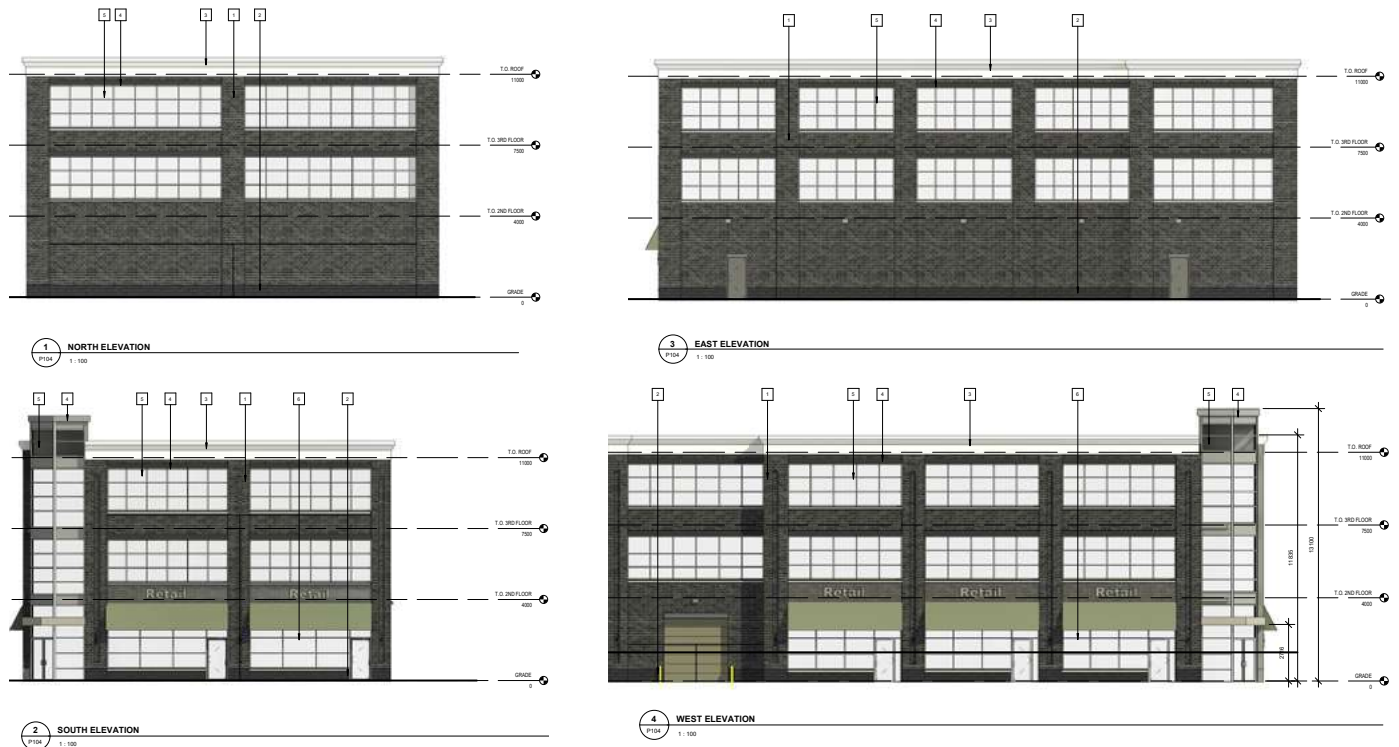


Figure 7.10 Building 04 Elevation



Figure 7.11 Building 05 Elevation

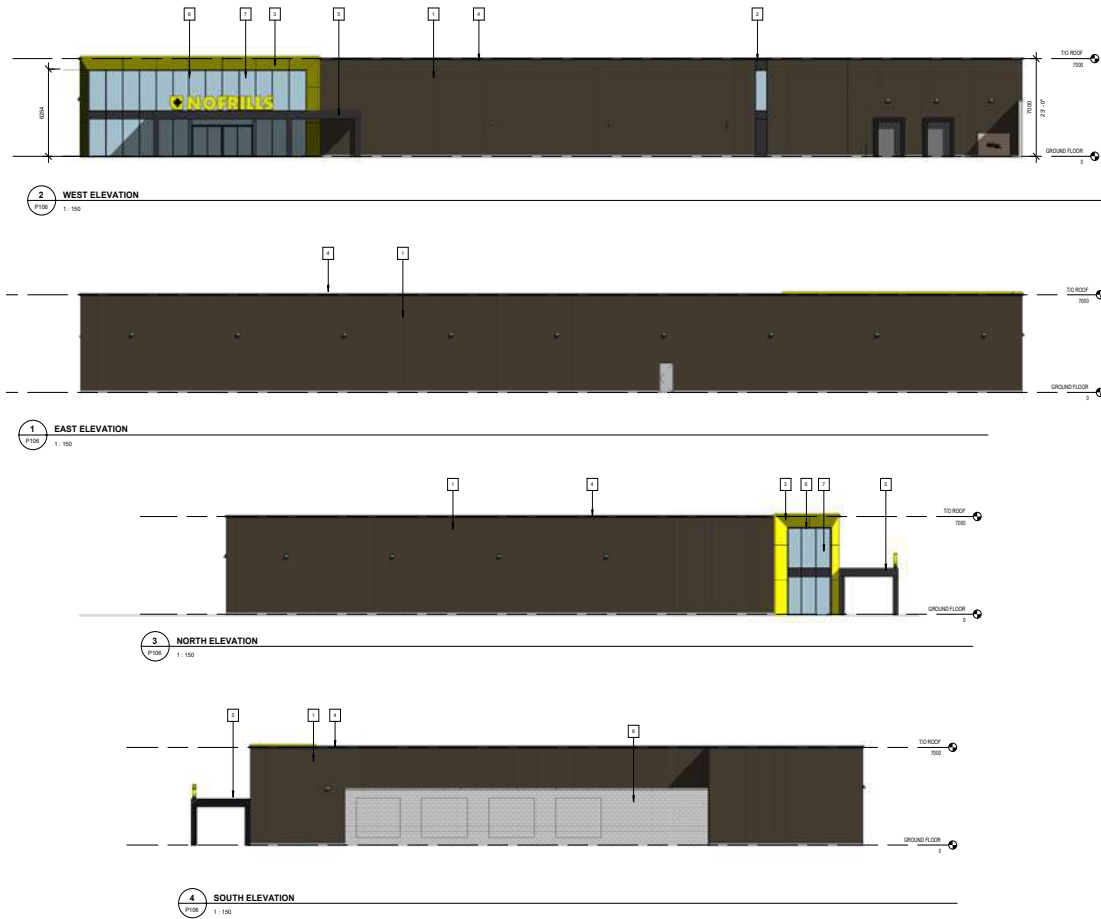


Figure 7.12 Building 06 Elevation

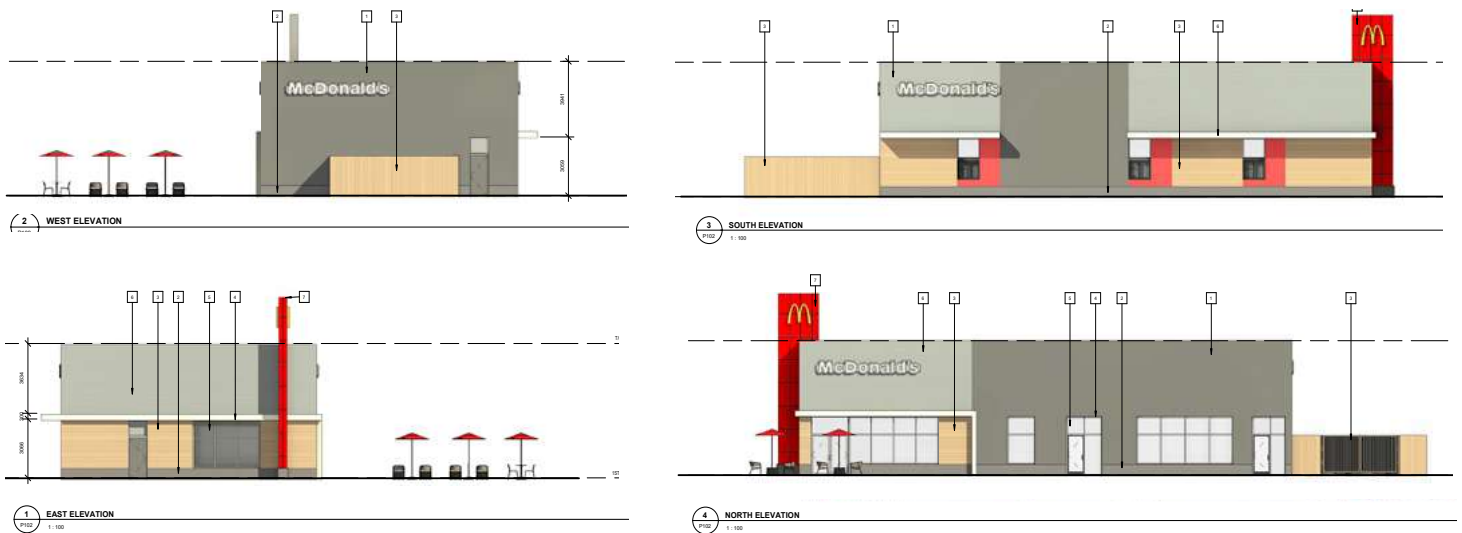


Figure 7.13 Building 07 Elevation

details, and window sizes to create visually appealing elevations. Windows and doors on primary elevations are configured in a consistent rhythm to maintain continuity and enhance pedestrian interest. Fenestration in other directions establishes a consistent and comparable elevation in relation to the primary active facades, avoiding blank wall conditions through material changes and architectural detailing. However, where commercial/mixed-use buildings (04, 05, and 06) are adjacent to residential areas to the east, rear façades will feature blank walls to prevent overlooking and protect the privacy of neighbouring residents.

Retail fronts for the mid-rise building are designed with a high level of transparency through materials and large display windows, especially near activity nodes like primary entrances and patios, while the upper-level façade will be articulated with a combination of vertical and horizontal

elements, cantilever and inset balconies, fenestration details, and material variation to create aesthetic interest and reduce the visual impacts related to the scale of the proposal. This includes creating visual appeal, breaking up the mass and avoiding blank wall conditions

Primary entrances are all buildings are emphasized architecturally, featuring taller elements, extensive glazing, and decorative treatments such as canopies, landscaping, and signage to distinguish them from other façade elements and highlight key storefronts. These entrances will be oriented toward and visible from adjacent streets or main circulation routes.

Additionally, weather protection features will be incorporated along the building façades with active uses to shield pedestrians and users from weather conditions all-year around. The windows will be of high quality, low

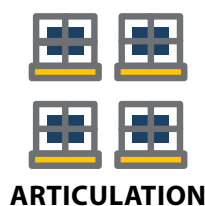


Figure 7.14 Proposed building facade

8.0

ACTIVE TRANSPORTATION

UDM SEC. 8

A. Pedestrian Circulation

1) Developments shall provide necessary infrastructure for pedestrian accessibility to support safe and convenient movement of people.

3) Sites shall incorporate a pedestrian path network that links uses to each other and permits pedestrians to move in as straight line as practical to their destination. Pedestrian linkages shall specifically be required between the following: a. Transit stops; b. Sidewalks; c. Trails; d. Building entrances; e. Parking areas;

7) Pedestrian ways and connections shall be designed to be convenient, comfortable, safe and easily navigable, continuous and barrier-free. All pedestrian ways shall meet the following: a. Be clear of obstructions, maintaining a minimum 1.5m wide passageway and 2.4m clearance above grade; b. Be slip resistant; c. Be unobstructed and without unnecessary meanders around built obstacles such as mail boxes, street lights, utility poles, seating, and street furniture; d. Be hard surfaced (hard-packed gravel may be permitted for walkway surfaces in areas adjacent to natural areas); e. Appropriate accessibility components and design for persons with disabilities shall be integrated into the overall pedestrian circulation system including ramps and sight assistance strips and textured edges at grade transitions and street crossings for example; h. Be distinct from vehicle lanes; i. Provide/maintain continuity of pedestrian ways from adjacent and/or existing ways;

11) All streets and major entry thoroughfares for developments (as per UDM Section 3-Streets) shall provide sidewalks on both sides as per the following: a. The alignment of the sidewalk shall remain straight along the street; b. Sidewalks shall be separated from adjacent streets by landscaped boulevards with street trees (where right-of-way permits).

13) For non-residential uses, walkways should be provided outside of all main and secondary entrances and around buildings to link these.

14) The primary method to create physical barriers from moving vehicles for pedestrian areas shall be trees with landscape beds, and/or raised planters. Decorative bollards, pavement markings, and signage shall be used as secondary methods only upon approval of the Town.

B. Enhanced Pedestrian Connection

3) Required pedestrian connections shall be aligned with street ends, laneways or other pedestrian walkways where practical.

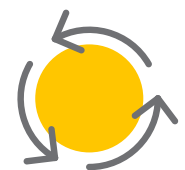
4) Required pedestrian connections shall be designed to meet the requirements of UDM Section 10-Landscaping & Public Spaces.

RESPONSE

The proposed site design incorporates an integrated network of active transportation comprising pedestrian walkways and crosswalks to and from the surrounding sidewalk systems. This multi-modal mobility pattern provides balanced and barrier-free access to and from the site and meets the needs of pedestrians and cyclists travelers as well as visitors driving through the site.

The proposed network links the proposed development to the surrounding public sidewalks and the Hamilton Drain Trail to the north, as well as the new active transportation route along Hurontario Street and Poplar Sideroad while tying up indoor and outdoor spaces within Site. A 3m concrete sidewalk with cart corrals on both sides bisects the central surface parking lot, providing safe and direct west-east access across the proposed development. Direct pedestrian access will also be protected for and from the intersection of Hurontario Street and Poplar Sideroad to building 08, again connecting the site to the public realm.

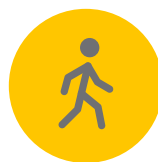
The proposed movement pattern prioritizes pedestrian traffic within the site by creating a well-knit, well-defined, and safe circulation system with minimal conflict with vehicles. The site design incorporates both hard and soft landscaping features along the proposed paths to create a safety buffer and a visually appealing pedestrian environment at street level. Landscape beds between parking aisles also provide refuge areas for pedestrian movement within parking areas. As mentioned previously, the proposed landscape areas along the public sidewalks will be elevated, with decorative metal fencing to create an animated streetscape. Primary building entrances and active facades are oriented to frame and enhance the pedestrian realm.



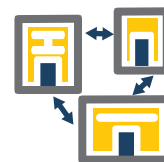
CIRCULATION



ACCESSIBILITY



PEDESTRIAN-ORIENTED



CONNECTIVITY



Figure 8.1 Proposed pedestrian and vehicular route

The proposed pedestrian sidewalk will be marked clearly with high-quality and durable hard surface pavement material. Sidewalk material will be integrated into the common theme and architectural style of the site. To protect pedestrians from weather conditions and create an interesting storefront window, the building design will incorporate weather protection features such as awnings and overhangs to create comfortable and all-season frontages. Signage and lighting features will further assist

with site navigation and wayfinding.

Furthermore, the proposal includes a total of 59 bike parking spaces, strategically distributed near the main entrance of each building to encourage active transportation and provide convenient access for cyclists. This integration of bike parking supports sustainable mobility options and enhances connectivity between the Site and a broader context.

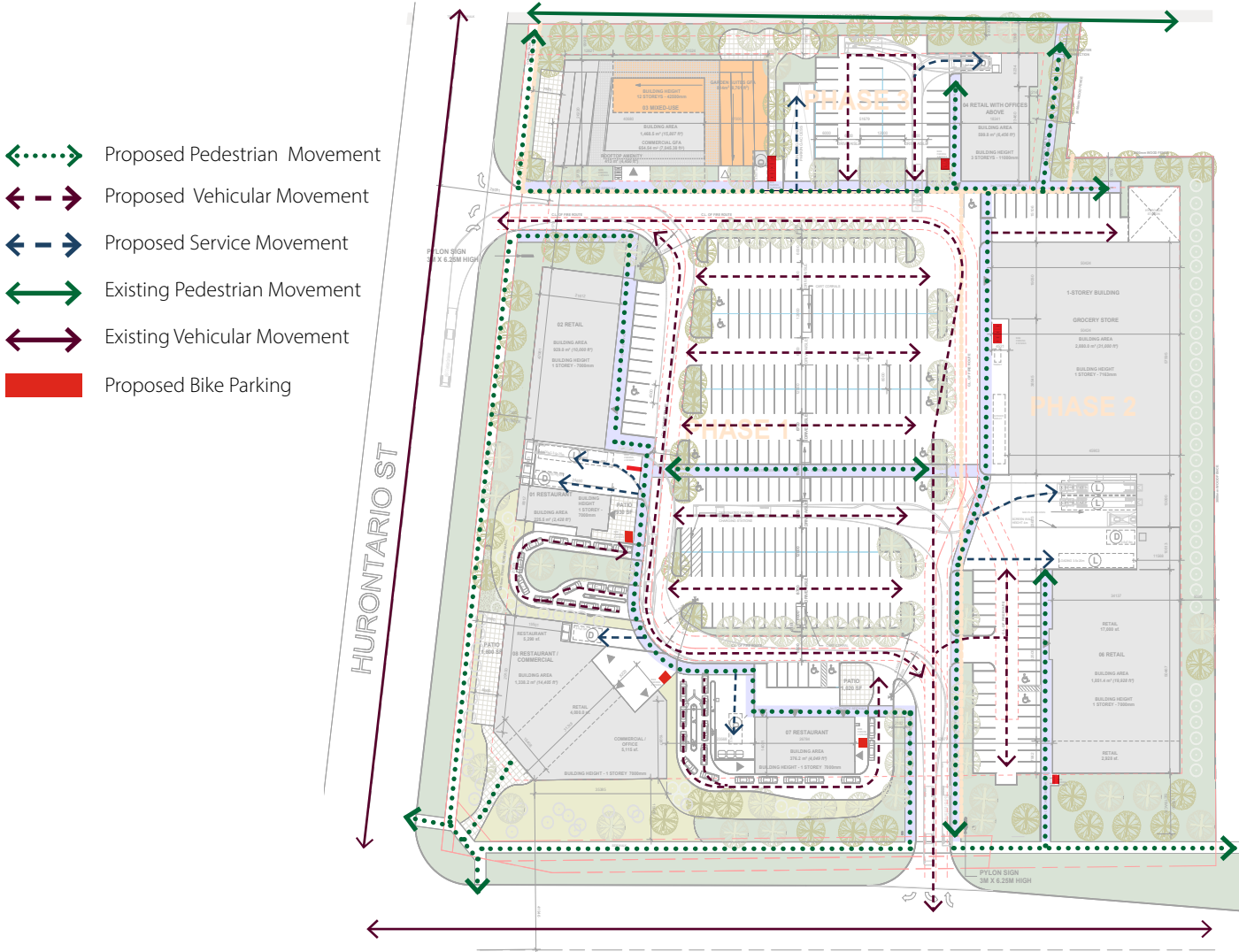


Figure 8.2 Vehicular and pedestrian circulation

D. Street System

- 1.** Street corners may be required to incorporate curb extensions to provide increased pedestrian space; safe refuge for pedestrians while waiting to cross the street; reduce street-crossing distances; and, opportunities for street furniture.
- 2.** Sight triangles shall be kept free from visual obstructions to facilitate safe vehicle and pedestrian movements.
- 4.** There shall be minimal or no change in the elevation of sidewalks across private access driveways.
- 5.** Where vehicle entrances cross sidewalks, advanced stop bars shall be provided on the site exit lane to improve pedestrian safety.
- 2.** Pedestrian walkways in parking lots shall be provided as follows: a. Pedestrians shall not be required to cross more than one pair of vehicle drive lanes to access a walkway; and, b. Parking stalls shall be a maximum of 24m from a pedestrian walkway.
- 3.** The arrangement of parking areas, primary maneuvering lanes, and main building entrances shall be designed to reduce the number of pedestrian/vehicle conflict points necessary for people to access the building. Site enhancements to reduce these conflicts may be required.

E. Parking Areas

- 1.** Pedestrian walkways shall be provided through all parking areas. These shall meet the following provisions:
 - a. Shall provide dedicated pedestrian walkway access throughout the entire parking area;
 - b. Be raised to sidewalk height. Lots with less than 100 parking stalls may utilize at-grade walkways based on design merit and use of enhanced markings;
 - c. Include clear surface markings to define pedestrian ways which provide clear and distinct visual identification along the entire walkway, using different paving materials such as concrete; inlaid pavers; or painted patterns;
 - d. Include sight assistance strips at all grade transitions; drive lanes; and, street crossings; and, e. Utilize shy spaces around planter strips and raised planters to provide refuge areas and safe travel routes for at grade portions of pedestrian walkways.

F. Trails

- 4.** Projects shall be required to provide extra wide, 3m sidewalks, along arterial streets and corridors where these are being developed/ used as combined bike and pedestrian ways for active transportation (such as along First Street for example)

F. Bicycle Circulation

- 1.** Infrastructure shall be designed to enhance and increase access for cyclists (bike ramps on staircases; and, cycling oriented signage for example).
- 6.** To support active transportation, all workplace settings/ buildings with 20 employee/occupants or more, shall provide:
 - a. Long term secure bike parking for employees; and,
 - b. Are encouraged to provide change rooms and showers, at least one unisex facility.
- 9.** Bike parking shall be designed to allow both the frame and wheels of a bicycle to be locked.

RESPONSE

The proposed traffic pattern is designed to reduce the expansive areas of asphalt that will result in uncontrolled vehicle movement. Walkways/sidewalks and landscape areas are provided to soften the edges of vehicular movement and to create an interconnected and easily navigable pedestrian/vehicular network that will prioritize active transportation.

The vehicular accessway is proposed with pedestrian connections at both sides of the entryway, and the proposed sidewalks remain aligned with the driveway at one side to promote connection across the site and accessibility to and from the proposed buildings and parking lots. Parking fields within the site are shallow, ensuring that walks from vehicles to main entrances are not expansive, thereby reinforcing a safe parking area.

Walkways also continue to wrap the buildings and provide linkage to primary entrances. On the site exit lanes, stop signs will be provided to improve vehicular and foot traffic safety. The proposed network is designed to break up

large parcels into smaller, more permeable areas within this shallow parking field.

As mentioned previously, the proposed dedicated walkway provides pedestrian linkage through all parking areas. The parking lots will use surface marking to clearly define vehicular circulation. Pedestrian sidewalks will be materialized to be visually and physically marked and defined. The proposed landscaping within the parking aisles, bays, and along the drive-through stack lane provides refuge areas for safe travel routes at the pedestrian level.

The proposed parking areas, for the most part, are located on the inferior sides of the buildings and are configured to have the minimum number of entry points from the primary maneuvering lanes to minimize vehicular and pedestrian conflict. The proposed loading and service areas are also consolidated and accessed from the rear or side of the buildings to reduce conflict with the pedestrian realm.



Figure 8.3 Proposed parking area with well-delineated pedestrian walkway

9.0

CAR-ORIENTED COMMERCIAL

UDM SEC. 9

A. Car-oriented Commercial

1. Automobile queuing lanes shall be located at the side or rear of buildings.
2. Drive-through queuing lanes shall be physically separated from parking areas with a 1.5m landscaped planter bed that includes sufficient landscaping to screen views to a maximum height of 1m.
3. Adequate queuing distances, based on expected use, shall be provided to prevent vehicle traffic from blocking streets, pedestrian ways, or onsite circulation.

C. Automobile Canopies & Bays

1. Drive-through, car wash and service bay structures shall be the minimum height necessary for their intended use.
2. These structures shall be compatible in design and materials with the architectural elements of the main building.

3. The primary street frontage shall not be dominated by the drive-through canopy or bay component of the development.

RESPONSE

The proposed buildings with drive-through facilities are internally programmed to ensure safe drive-through lanes without pedestrian conflict. The queuing lanes are separated from traffic aisles by a curved curb and landscaped planter beds, creating a clear and safe separation. These queuing lanes are positioned at the rear of the buildings, with landscape buffers to minimize visual impact from adjacent public streets and sidewalks. They are also designed to accommodate the required length for short stops without disrupting on-site circulation.

The drive-through facilities will be thoughtfully designed and articulated to seamlessly integrate with the overall site and building style. Vehicles will be able to safely exit the drive-through lanes and merge into traffic aisles without crossing pedestrian sidewalks. Weather protection, such as canopies, will be provided but will be focused on the buildings' primary frontages to guide users to main entrances and enhance the pedestrian experience.



Figure 9.1 Proposed drive-through facilities for Starbucks

10.0

LANDSCAPE AND PUBLIC SPACES

UDM SEC. 10

A. General Design

1. Developments shall create a landscape plan that through the use of a variety of plant species, and a mix of ground covers, shrubs, and trees, achieves the following as applicable to the proposal: a. Enclose spaces; b. Defines prominent features such as entrances and activity areas; c. Focuses attention on buildings and landmark features; d. Adds visual interest to the project and streetscape; e. Creates visual interest through seasonal variations in plant materials; f. Defines pedestrian zones; g. Defines public and private spaces and different uses; h. Adds to the general comfort and sun health of site users; i. Provides visual screens and buffers for incompatible uses; j. Defines the streetwall; k. Provides environmental benefits associated with air and water quality and habitat protection; l. Provides visual and physical linkages between various green spaces, open spaces, and outdoor activity spaces; m. Provides safe and effective play spaces; n. Improves the aesthetic qualities of parking areas and stormwater management facilities; o. Provides comfort for users of outdoor spaces; and, p. Provides a continuous landscaped connection between buildings, streets, parking areas, and project boundaries.

4. All landscaped planter beds adjacent to vehicular areas shall be protected by a minimum 6-inch-high concrete curb.

5. Primary entrances for pedestrians, as well as those for vehicles, shall be emphasized by landscape features that use plant materials with a greater variety of seasonal interest, texture and colour compared to the rest of the site design.

6. As much as practical, landscaping shall limit environmental impacts and enhance habitat through: a. The use of hearty native plants and non-invasive plants; and, b. Minimize or eliminate the need for irrigation.

7. Plant, shrub, and tree species shall be appropriate to the general climate zone for Collingwood, and specific micro climate conditions for the site. The accepted Canadian Hardiness Zone for Collingwood is Zone 5a.

16. Landscaped areas shall be designed to be protected from damage by vehicles, with features such as raised curbs, and/or decorative bollards.

B. Planter Beds

1. Planter beds without trees shall be a minimum of 1.5m wide.

2. Planter beds (including boulevards along streets) with trees shall be a minimum of 2.4m.

3. Planter beds/islands located within a parking area (separating drive lanes) shall be at least 3m wide.

D. Frontage Planting

1. Building setback areas shall be fully landscaped to provide visual interest, accent the building architecture and bring a sense of nature into the urban environment, excepting areas used for the following: a. Infrastructure for active transportation; b. Public art; c. Stormwater facilities that are designed as an aesthetic amenity; d. Outdoor amenity spaces; and/or e. Outdoor seating for restaurants, cafés, or similar uses; and, f. areas used for vehicle lanes and parking.

E. Perimeter Plantings

2. For non-residential uses, a 2m landscape perimeter may be required along side yard lot lines, from the rear edge of the building to the rear property line (where this does not disrupt other site functions) and shall be designed as an attractive buffer.

F. Foundation Planting

1. 2m wide foundation planting beds shall be provided along all buildings. These shall be landscaped to provide visual interest and should complement the building(s) and entire landscape plan for the site. Sides of buildings not visible from public areas, such as loading areas, are exempt. Other sides of buildings may be exempt based on overall design merit. Uses, such as storefronts, may be exempted based on design merit and practicability.

G. Tree Canopy

1. All sites shall provide sufficient tree cover to create tree canopy shade over at minimum of 30% of the site area, with particular attention to hard surface areas. This includes large parking lots. Tree canopy size is calculated at maturity.

I. Screening

1. The design considerations for buffers and screening of nuisances or unwanted impacts shall meet the following: a. The buffer/screen shall be appropriately located to most effectively obscure the view of the nuisances or unwanted impact; b. The depth of the buffer/screen mass shall be relative to the strength or magnitude of the nuisance or unwanted impact; c. Use a combination of plants; d. Grading shall be used to enhance the effectiveness and visual interest of the buffer where practicable; e. The height of the screen is as important as its width or depth and shall be designed accordingly in terms of screening nuisance or unwanted impacts; and, f. The buffer shall be designed to be aesthetically pleasing.

3. Screening may be achieved through any combination of the following as approved by the Town: a. Use of landscaping with coniferous and deciduous species that restrict visual access from affected areas; b. Decorative fencing and wall combinations; c. Fully enclosed building additions; d. Partially enclosed building additions that house the use(s) being screened; e. Free standing partial enclosures; and, f. Land forms such as berms and rockeries;

g. Existing vegetation may be integrated into screening designs if fitting with the overall landscape plan.

7. Vehicle storage, mechanical equipment, trash and recycling receptacles, product storage, and service areas are to be screened from streets, parks, public spaces, and pedestrian walkways.

J. Parking Lot Landscaping

1. A 3m-wide perimeter planter bed for screening & trees shall be required around parking areas with the following minimal requirements: a. Shade trees shall be spaced at 10m intervals; and, b. Screening which provides a visual barrier with a maximum height of 1.0m to promote personal safety.

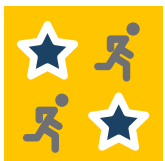
2. Surface parking lots that take up a portion of the street frontage shall increase the perimeter planter bed to 4.0m in width along the street frontage, and in turn the amount of landscaping materials.

3. A 3m-wide landscaped planter bed shall be provided between alternating rows of parking stalls (i.e. for every fourth parallel row of parking). These shall be landscaped and include shade trees planted at 10m intervals; and shall be combined with pedestrian walkways where practical.

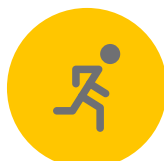
RESPONSE

The proposal provides enhanced landscaping elements, including soft and hardscape features, metal fences, piers, patios and seating areas, to enhance overall site functionality and add visual interest to the development. The proposed site design offers a landscape area along the primary street frontages, including Hurontario Street and Poplar Roadside, to enable the transition to the public realm, create a visually appealing streetwall, and enhance the aesthetic quality of the surrounding streetscape. These areas will incorporate softscape features such as planter beds, sod, trees, and shrubs to soften the site's boundary.

Furthermore, high-quality landscape designs are provided along the northern and eastern property lines. A 3-meter high wooden fence is located along the eastern property line to add an extra layer of protection and separation, ensuring potential impact from commercial and operational activities is mitigated. The proposal fosters a buffer that minimizes overlook opportunities and protects the privacy of both the residential area to the east and the 12-storey mixed-use residential building.



CHARACTER



ANIMATION



PUBLIC REALM



GATEWAY

Wherever surface parking areas and delivery spaces are proposed adjacent to public streets or paved trails, landscape elements will be provided to soften the edges and create a separation. For example, perimeter planter beds and plant materials will serve as a barrier, minimizing the visual impact on the public realm. The proposed parking areas and drive-through lanes are buffered through landscape strips to create a physical and visual barrier between vehicular circulation and the pedestrian and activity realm. Landscape islands/bulbs are proposed between parking aisles to provide a refuge area for pedestrians moving within the parking lots.

The Arborist Report prepared by C.F. Crozier & Associates Inc. states that the 30% tree canopy requirement under the Town's UDG is obtainable.

The proposal will seek to use vegetative species that are native, non-invasive, and tolerant to urban conditions such as drought, salt, heat, and disease, with low resource

maintenance requirements. The proposed landscape trees and plant species will be selected based on the appropriate Hardiness Zone, year-round climatic conditions, and visual appearance to promote a sense of place and the characteristics of the area.

Changes in paving material and pattern will be used to mark essential site thresholds and programmatic changes. This approach aims to improve the overall legibility of the development, effectively distinguishing different activities such as pedestrian sidewalks, crossings, and patio areas. Moreover, surface finish materials will be selected to display harmonious texture/ patterns aligning with the area's visual definition.

In all, the design will be further defined through a balance between soft and hard landscaping, as well as a dynamic interplay of textures and functions at various levels, enhancing the edges and key locations of the site and creating diverse user' experiences.



Figure 10.1 Render showing the proposed landscape areas along the property line

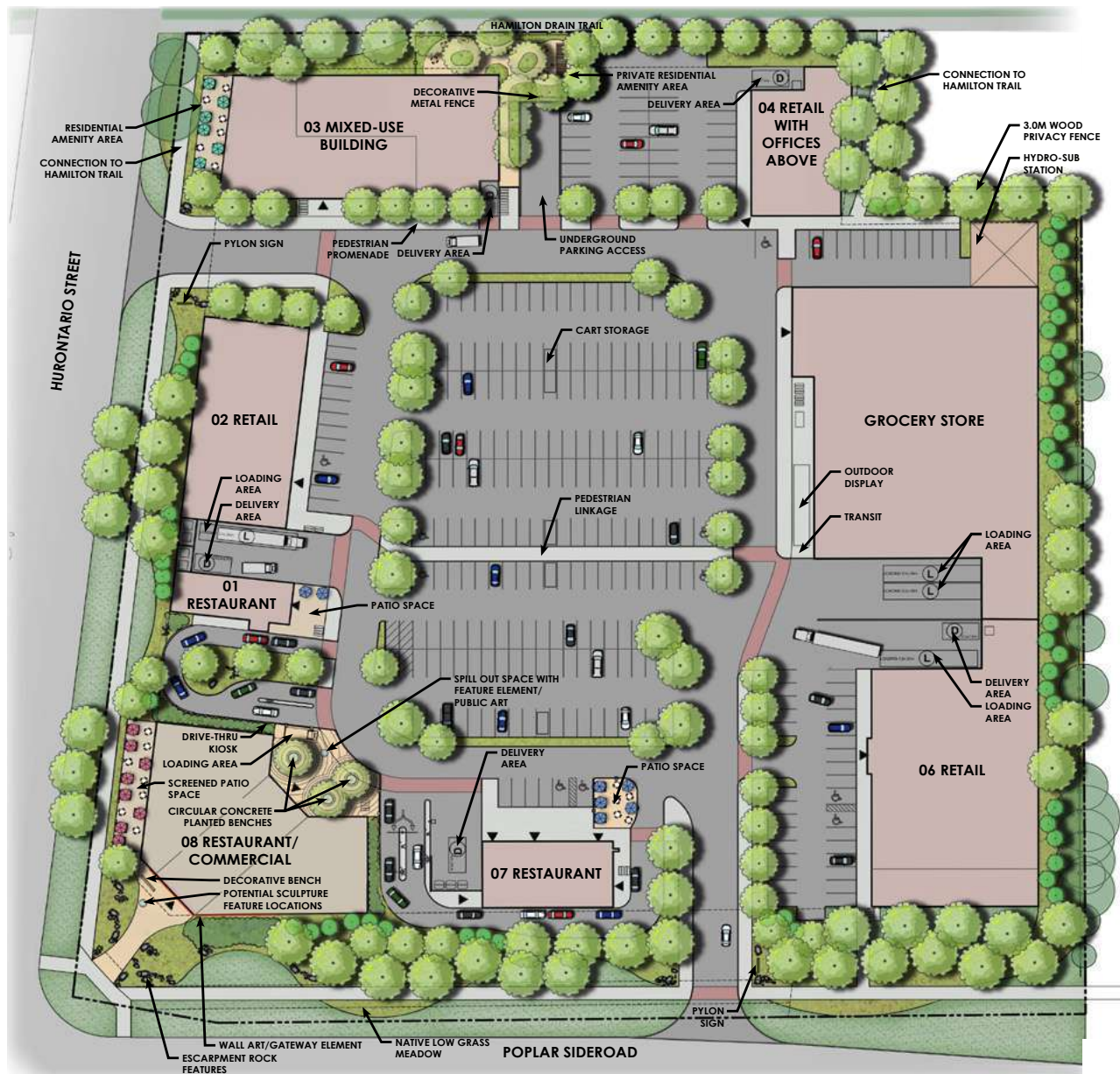


Figure 10.2 Landscape Plan

11.0

CONCLUSIONS

The proposed mixed-use complex development represents intensification and investment within a prominent Highway Commercial site at Collingwood's central gateway location. The proposed development reflects the future planned context for the area and is located near planned commercial development and established residential areas. The proposed development will provide a range of services, residential, and employment opportunities that will assist the Town of Collingwood in supporting housing supply and meeting residents' needs while fostering a regional service center that serves neighbouring communities and visitors.

The proposed site layout, architectural design, and landscaping have been carefully examined to complement and enhance the existing and future Lakeside and Mountain Recreational vernacular, along with the streetscape and public realm to define the landmark character commensurate with the central gateway. The proposed commercial expansion also supports the adjacent residential, commercial, amenity uses, and trail systems by providing service accessibility and enhanced pedestrian linkage. The proposed pedestrian walkways will create an animated, walkable, and connected pedestrian environment within the site and promote wayfinding in the surrounding neighbourhoods and across the area.

The proposal will utilize both high-quality architecture and landscape design that complement its context and will create a visually appealing, safe, and appropriately scaled public street environment and gateway that is welcoming to residents and visitors. The proposal will be compatible with the planned context for the Town of Collingwood and will assist the Town in achieving its growth and development objectives.

Based on our review, it is our opinion that the proposal adheres to the design direction provided in the Town of Collingwood Urban Design Manual. Overall, it is our opinion that the proposal represents a good design that will enhance placemaking at this gateway location.

12.0

DESIGN TERMS



ACCESSIBILITY
Providing for ease, safety, and choice when moving to and through places



ACTIVE TRANSPORTATION
The use of human-powered transportation as alternative to motorized-transportation



ADAPTIVE REUSE
Converting an existing building uses into a new use



ANGULAR PLANE
A geometric measurement that maintains solar access and height transition



CHARACTER
The look and feel of an area, including activities that occur there



CIRCULATION
The movement patterns of people and vehicles through a site or community



COMPATIBILITY
Ensuring the size, form and character of a building fits relative to others around it



CONNECTIVITY
The ease of movement and access between a network of places and spaces



FINE GRAIN
A pattern of street blocks and building footprints that characterize an urban environment



FOCAL POINT
A prominent feature or area of interest that can serve as a visual marker



GATEWAY
A signature building or landscape to mark an entrance or arrival to an area



HEAT ISLAND EFFECT
Buildings and paved surfaces that retain and re-emit the sun's heat, resulting in higher temperatures in urban environments



MASSING
The effect of modifying the height and bulk of the form of a building or group of buildings



MAJOR TRANSIT STATION AREA
Areas within walking distance of an existing or planned higher order transit station



MICROCLIMATE DESIGN
Design strategies that create comfortable outdoor conditions for year-round use



NATIVE PLANTING
Plants from the same local ecology, used to improve biodiversity, reduce levels of maintenance and conserve water



PUBLIC REALM
Public spaces between buildings including boulevards and parks; where pedestrian activities occurs



RHYTHM AND PATTERN
The repetition of elements such as materials, details, styles, and shapes that provide visual interest



SETBACK
The orientation of a building in relation to a property line, intended to maintain continuity along a streetscape



STEP BACK
A recess of taller elements of a building in order to ensure an appropriate built form presence on the street edge



TRANSIT-ORIENTED COMMUNITY
Compact, mixed-use, pedestrian-friendly developments near public transit



TREE CANOPY
Cover and shade created by the layering of deciduous tree branches and foliage



URBAN FABRIC
The pattern of lots and blocks in a place



VIEW TERMINUS
The end point of a view corridor, often accentuated by landmarks



ANIMATION

Support sustained activity on the street through visual details, engaging uses, and amenities



ARTICULATION

The layout or pattern of building elements (e.g. windows, roofs) that defines space and affects the facade



BARRIER FREE

Public and private places and spaces, designed to accommodate persons of all ages and abilities



BUILT FORM

The physical shape of developments including buildings and structures



DESIRE LINE

Shortest or most easily navigated route marked by the erosion of the ground caused by human traffic



ECOLOGICAL RESTORATION

Strategies to enhance existing natural heritage systems for environmental benefits



FACADE

The exterior wall of a building exposed to public view



FIGURE GROUND

The visual relationship between built and unbuilt space



HEIGHT TRANSITION

The gradual change in height between buildings within a community



INFILL DEVELOPMENT

Development of underused lands within existing built communities to complete or densify those communities



LANDMARK

Highly distinctive buildings, structures or landscapes that provide a sense of place and orientation



LANDSCAPE BUFFER

Enhanced landscaping along property perimeters that protect privacy and promote compatibility



NODE

A place where activity and circulation are concentrated



PASSIVE SOLAR DESIGN

Building design and orientation that utilizes the sun to promote greater use of renewable energy and building comfort



PASSIVE SURVEILLANCE

Design techniques to enhance visibility and safety of public areas



PEDESTRIAN-ORIENTED

An environment designed to ensure pedestrian safety and comfort for all ages and abilities



STREET ENCLOSURE

The ideal ratio of street to building wall that promotes a walkable and comfortable pedestrian realm



STREET FURNITURE

Municipal equipment placed on streets, including light fixtures, fire hydrants, trash receptacles, signs, benches, mailboxes, news-paper boxes and kiosks



STREETWALL

The consistent edge formed by buildings fronting on a street



SUSTAINABILITY

Developing with the goal of maintaining natural resources and reducing human impact on ecosystems



VISTA

Direct and continuous views along straight streets or open spaces



WAYFINDING

Design elements that help people to navigate through an area (e.g. signs, spatial markers)



URBAN INTENSIFICATION

Increases urban density and land use



WATER MANAGEMENT

Management of available water resources to promote water quantity, and its efficient use

